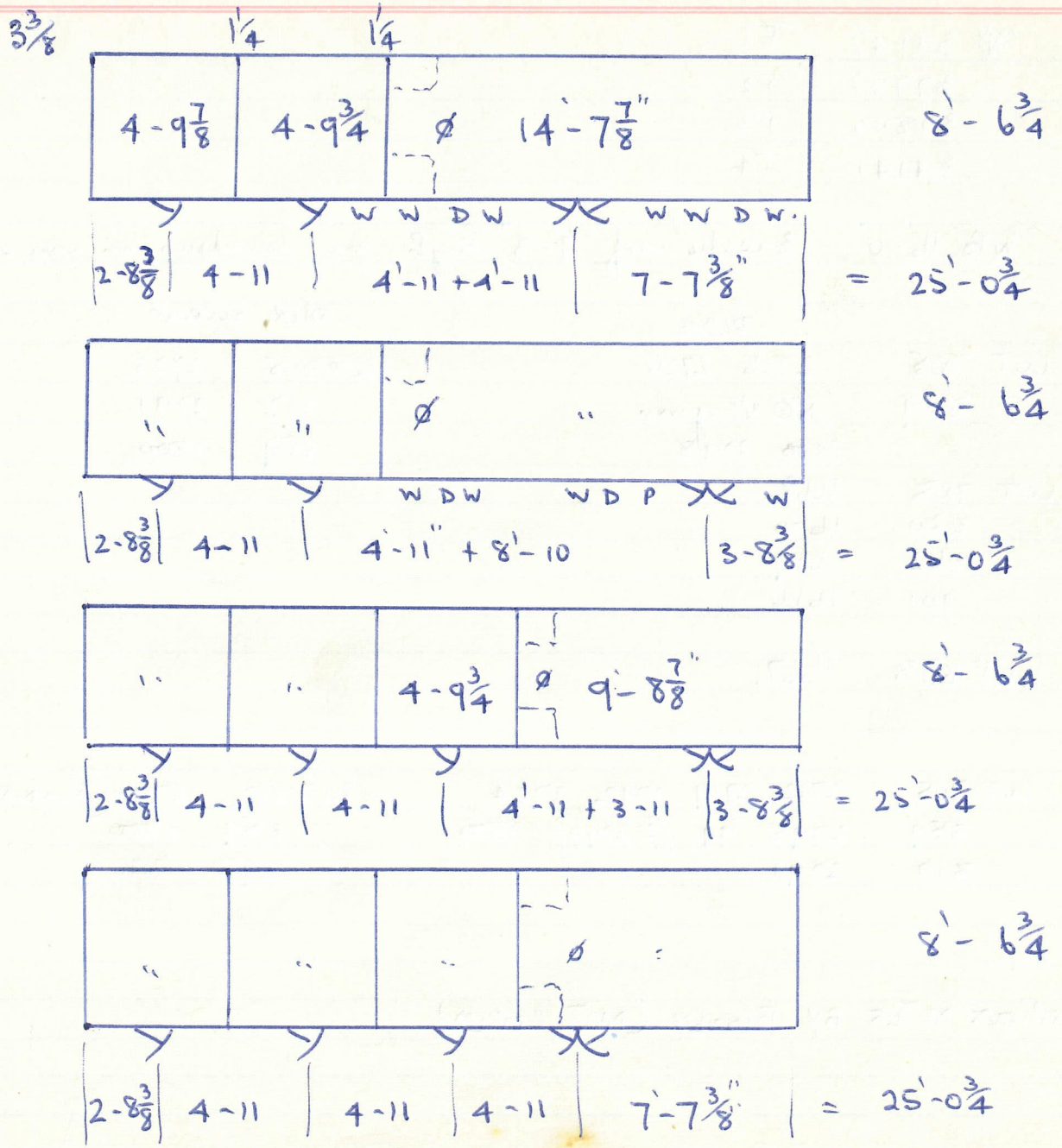
These 4 are conversions of S.17

T1
T2
T3
T4



P.T.O.

1/4" DIAG. No 34147. T1
37757 T2.
37800 T3.
34141 T4

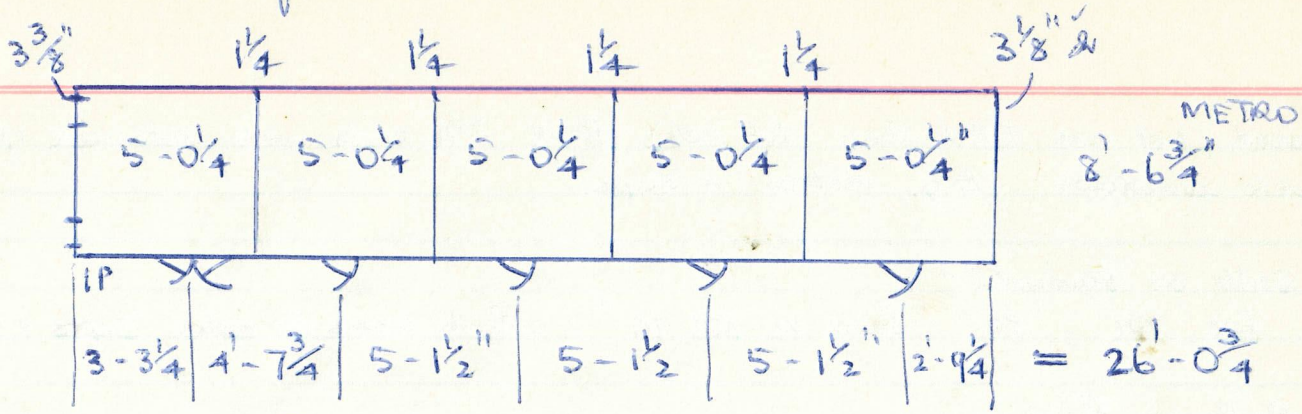
4W. WB 16'-0" 3 centre roof 7'-3" high no windows in rom ends.

		DIAG	OTHER SOURCES	
T1	LOT 705	2716 2726	LOT 705	2723.
	859	NO No given	817	2791
		PROB. 2798	859	2800
T2.	LOT 708	1668		
	820	1670		
	862	1669		
	901	1666		
T3.	LOT 878	1667.		
T4.	LOT 705	2712 2719 2792 2724	LOT 817.	2791 (later to T1)
	859	2798 "99" [? 2799] 2800	859	2800 " " "
	817	2791.	898	340

PLUS. COPIOUS NOTES BY BINNEY (NOT COPIED)

Both these are conversions from a 4-P.T. DESIGN FOR WHICH NO DIAGRAM SURVIVES.

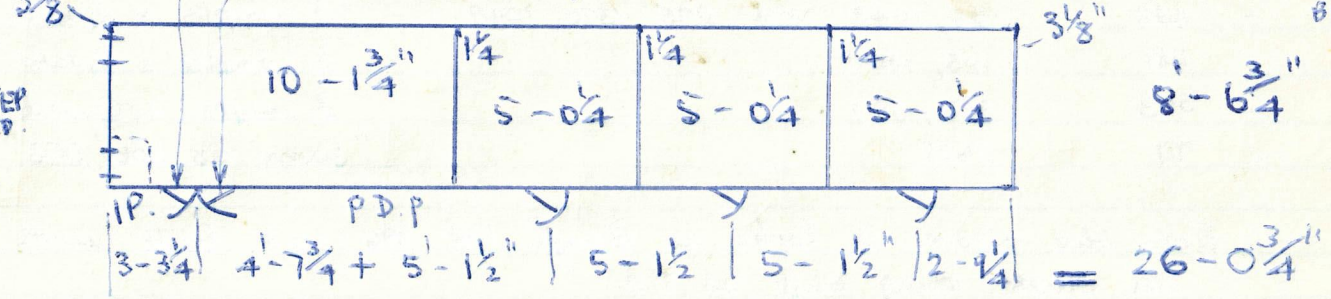
T.5.
T.8



METRO
8' - 6 3/4"

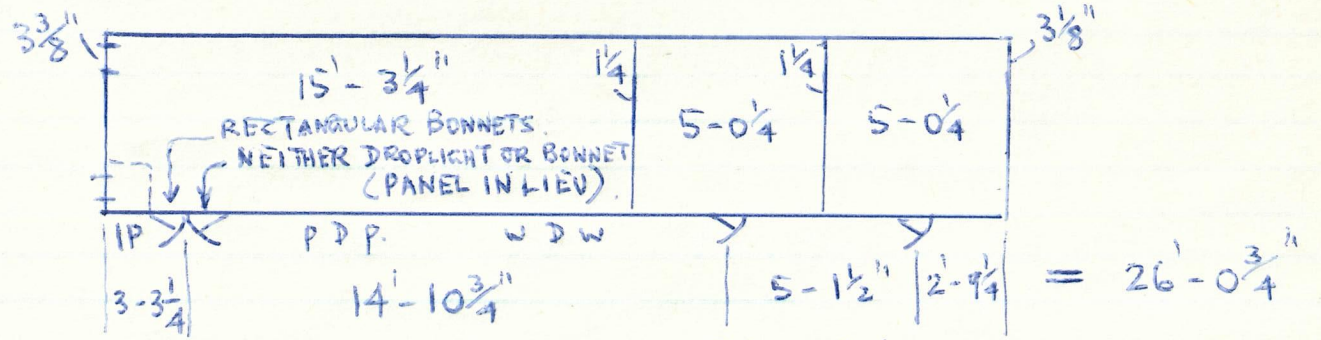
ROUND TOP DOOR WITH BONNET LH.
RH. SQUARE TOP DOOR, PANEL IN PLACE OF DROPLIGHT & CLER. TYPE OF LIGHT IN PLACE OF BONNET.

T8 ✓ STEP END.



8' - 6 3/4"

T5 ✓ &



1/4" Diagrams. — has not survived [Lots 396, 428, 481, 503 & 717] But see below.

T.8. No 4172

T 5. No 4174

4 wheels, wheelbase 17.0. 3-centre roof. Body 7.3 high. Windows in vau ends. NOTE that during the period when these 3 1/8 ends were in use (starting with Lot 390 and ending with a Lot not at the moment exactly

T5.

T.8.

traced, but not later than Lot 633) thick 3 3/8 ends were retained if there were windows. E.g. Metros & Slips.

T.8. Built as Second:-

Lot 721 80. "Same as Lot 717": which seems to mean 3cpts & Gd.

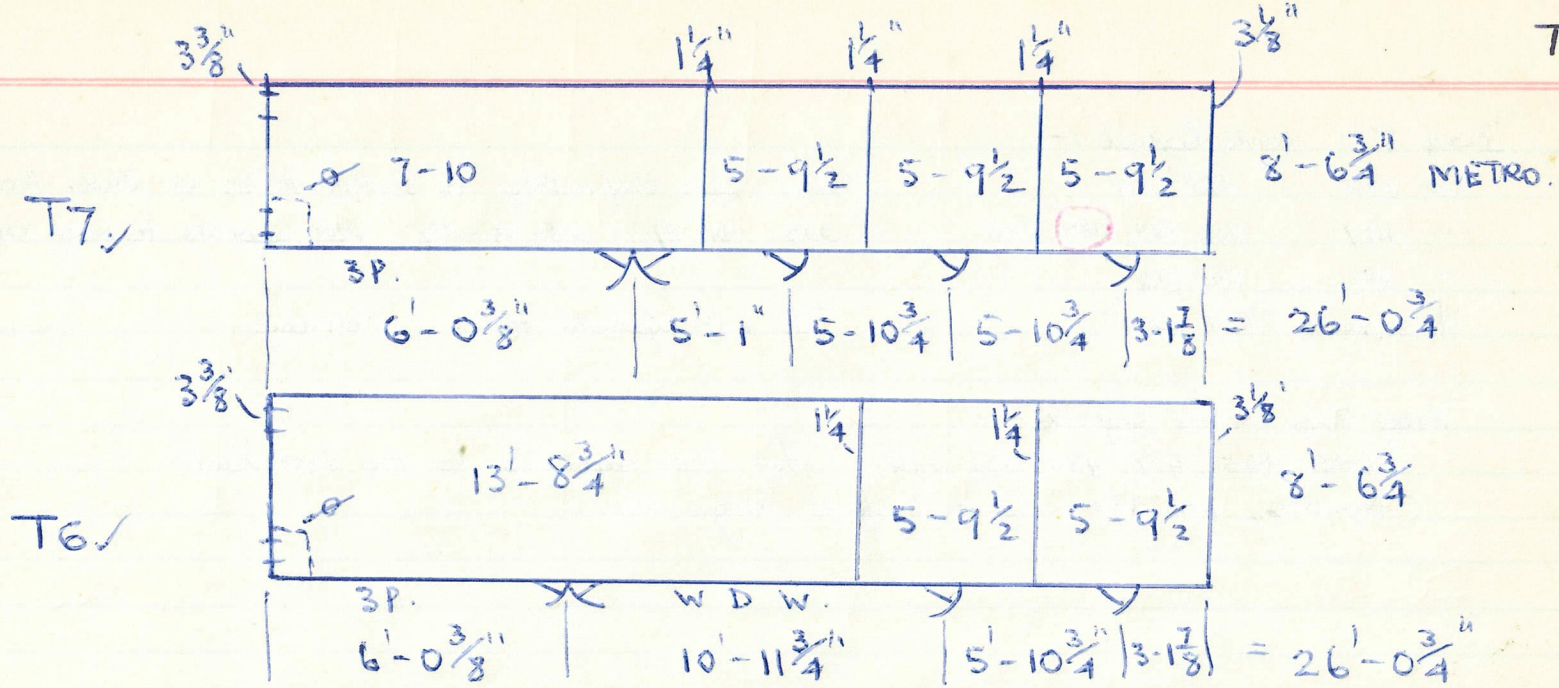
Built as Third:-

Lot	396	704, 777	"4 cpts & Gd."
"	428	54, 1043, 1334, 295, 1370, 1049	"3 compts & Guard"
"	481	155, 1200	"3 compts & Guard"
"	503	387, 399	"3 compts & Guard"
"	717	2628	"Same as Lot 503"

From the above it looks as though Lot 396 were built with four cpts. all others being built as T.8.

T.6.

T.7.



T.6 is a conversion from T.7. originally Brake Second.

1/4" Diagrams:- T6 No 41184
T7 No 41178

4 wheels, wheelbase 17.0. 3-centre roof Body T.3 high. Windows in van end. Rectangular weather bonnets over the double doors.
Lots 395, 427, 480, 502.

Lot	395	Bke	Second.	28, 31
"	427	"	"	24, 26, 27, 29
"	480	"	"	30, 33
"	502	"	"	37, 40.

For aux.(?) notes see overleaf.

T6 - T7.

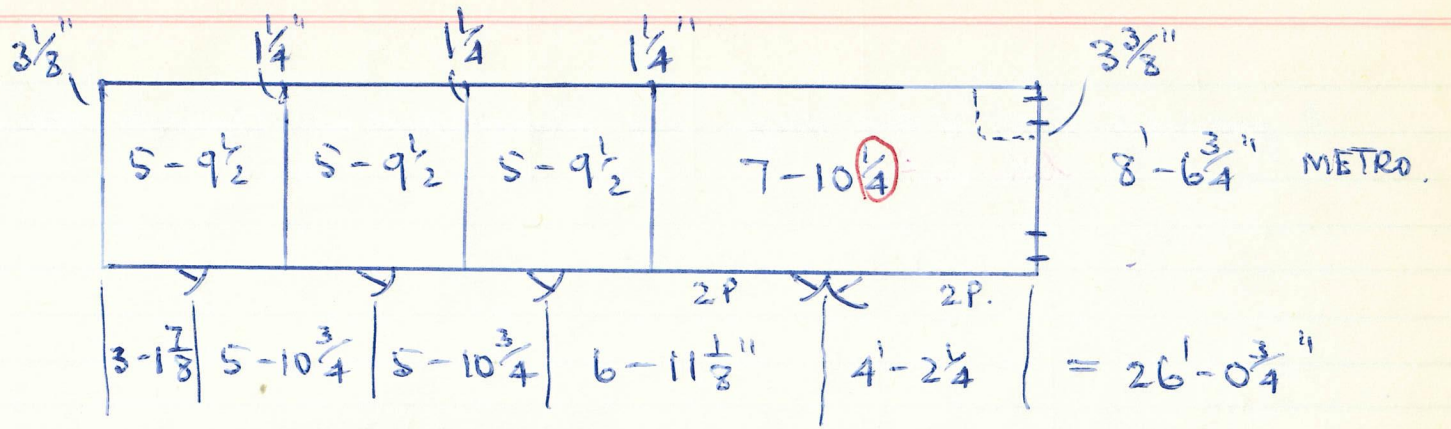
Diag T.7 beats legend :-

Lot 395	No 309	This can definitely be stated to be ex. 2nd. No 28.
" 427	Nos 228, 239, 286	228 ex-27 286 ex-29 239 appears to be an error.
" 480	No 262	
+ added "Lot 428"		This Lot appears to be irrelevant

Diag T.6 beats legend :- .

Lots 395, 427, 480, 502 & 721" Lot 721 appears to be irrelevant

" No 296, Lot 480, Long Buffers, Aug. 1931.



1/4" Diagram No 39699 A.
4 wheels, Wheelbase 17.0. 3-centre roof (presumably 7.3 high) Windows in van end.
Lots (alleged) 427, 480, 699.

This diagram presents some problems, and the fact that it bears an A number suggests that the tracing of No 39699 has been altered in some way. It resembles T.7. except in being (a) drawn (unusually) with R.H. van; (b) in (allegedly) having a van 1/4" shorter; and (c) in having the Double Doors almost central with relation to the van, instead of next the compt. partition.

Lot 427 appears to relate to B.T. 437 ex T.7 Second 24
Lot 480 " " " " B.T. 296 ex T.7. Second 30.

Lot 699 " " " " B.T. 316 ex S.18. Second 45 but if so to put a vehicle with 5.0 1/8 compts on a diagram with 5.9 1/2 compts seems heroic.

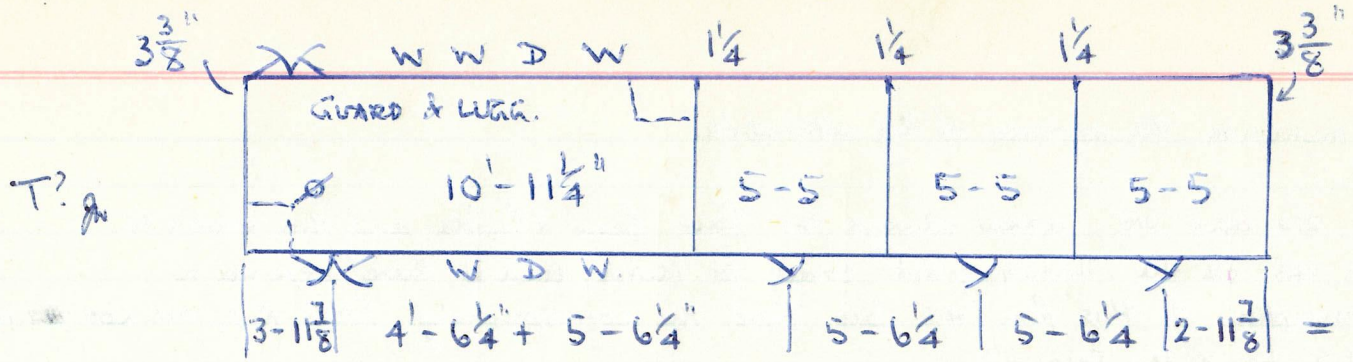
In all cases considerable reconstruction must have been involved in siting the Double Doors to existing bodies

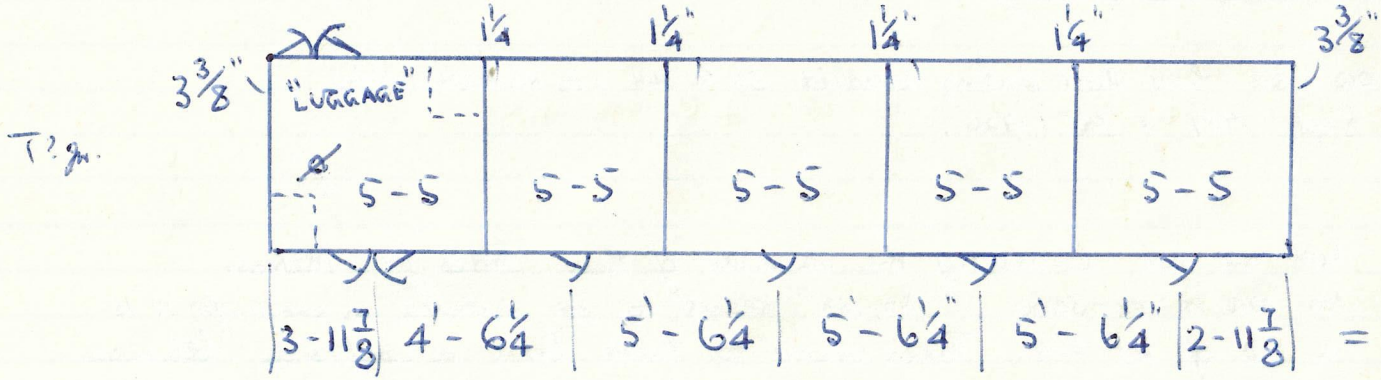
P.T.O.

A grounded body of this design was photographed at Gloucester 28/4/47
Who by?

These 2 Diagrams are conversions of 5/3RDS S2. (METRO 8'-0¾")

T10
T.13.





¼" Diagrams :- T10. No 41186. "Metro. Brake Third Lot 143. No 212" (MS. 215) Both independently confirmed.
T.13. No. 41067 "Metro Brake Third Lot 142. (MS 224. (ex :6) No. 153") conformed. apply error.

[also 227, 331, ex-2nds 7 & 2]

4 wheels W.B. 16-0. Arc roof 7.3 high. No horizontal end waist mouldings.
In each conversion the door of compt "A" has been retained, and a right hand leaf has been added, but by reason of the small length of the compts, this added leaf is less than standard size. The space available for it is equal to half the compt, less half the width of the original, retained, door; ie 2-8½ less 1-0, or 1-8½. This is the size of a similar door fitted to T42. On both sides the narrow door is fitted to the right of the retained full width door,

T.10.

T.13.

doubtless to minimise the expense of the alteration.

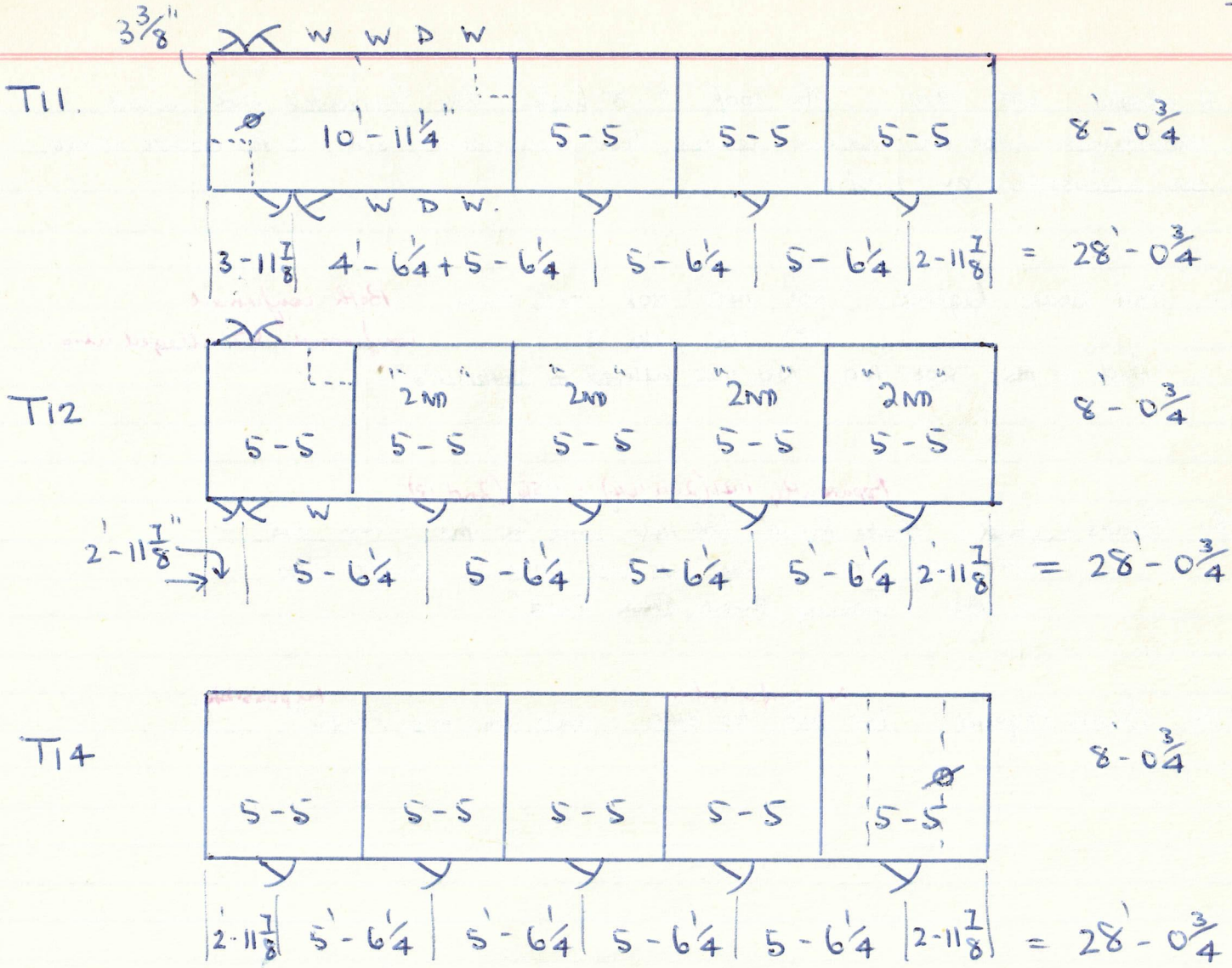
T10. Nos 212 and 215 appear always to have been Thirds and this seems to settle lot 143. as the Thirds of this Order for Metros, even if there were not a strong presumption of this from the fact that the corresponding Firsts R.1. are Lot 141 (lot 142, S.2, being the Seconds).

Note: 212 & 215 had their seating increased 30 to 44 & to 43 respectively Sept 1909 & Dec 1926.

T13. Lot 142 are evidently the Seconds of this Order for Metros. No 224 is ex-5006 "No 153" seems to be entered in error for it is a S.3 of Lot 224. There seems to have been a confusion between lot 224 of another diagram, and No 224 of this diagram.

ALL THESE ARE CONVERSIONS OF S4

T11. – T12.
T.14.



1/4" Diagrams :- T.11. No 41190.
T.12. No 41066
T.14. No 65298

T.11. – T.12.
T.14.

4 wheels. WB 19.0 Arc roof. 7'-3" high. No horizontal end waist mouldings. For the added leaves, less than normal size, to the double doors, see discussion on T.10/11.

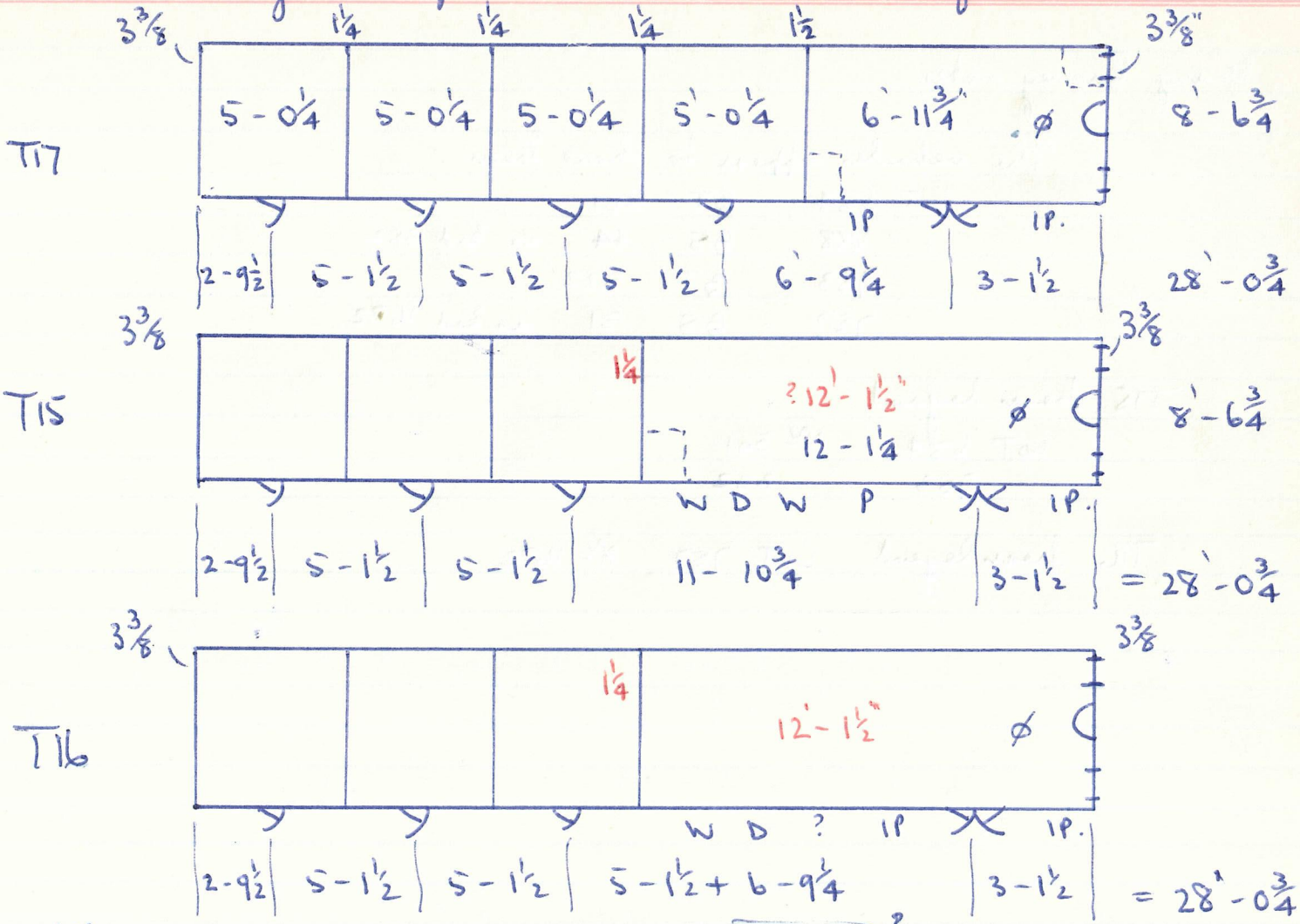
T11. This bears legend: "Lot 140 Nos 1140, 1146 Both confirmed.
Lot 175 No 3" confirmed. 62 alleged also.
And in ms: "Lot 140. No. 1140 altered to workmen's"

Apparently 1141 (2nd 164) & 1156 (2nd 18)
T.12. Bears legend: "Brake Second. Lot 140" and in ms. "140, 245, 298, 322, 175." These are all Lots on S.3/4 Also in ms "Tracing Shows Brake Third"

No confirmation Impossible.
T.14. Bears legend: "Lot 140 No 1140" and in ms. "1126"

T15
T16
T17

T17 is the original design. T15 & T16 conversions therefrom. 'METRO'



1/4" DIAG. T17. Nº 41181 LOTS 684 753 684 & in ms. 688
T15 41160 684 753.
T16 34146 " 757

4W WB 19'-0" 3-centre roof 7'-3" High. Windows in VAN end wall
The 1/2 doors forming the R.H. leaf of the DD's have neither skylights nor ventilators

P.T.O.

Many Brinney notes.

The vehicles appear to have been

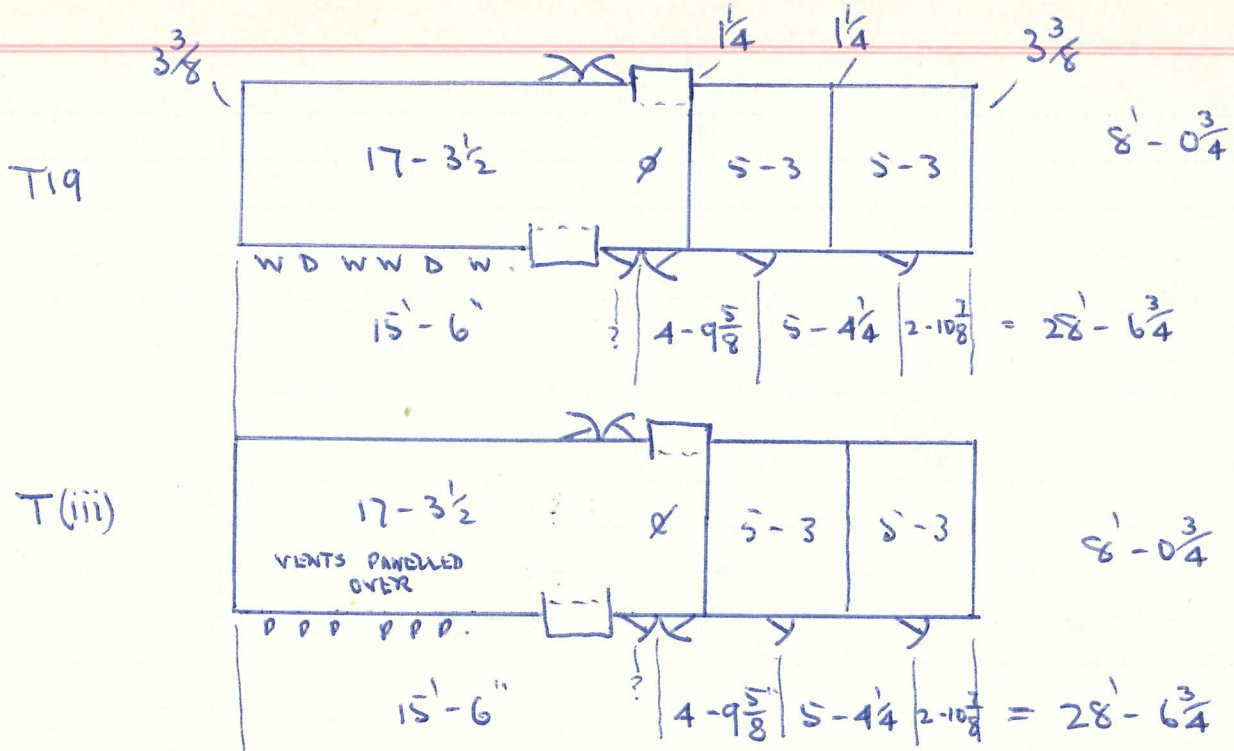
LOT 684	BT	301	
688	BS.	44	as 3rd 352
753	BT.	322.	
757	BS.	81	as 3rd 1672.

T15 been legend

LOT 684	No 301
753	322

T16 been legend LOT 757 No 1672.

T18
T19.



Conv. from T20 1/4" DIAG T18 60524 wingless version of T19. off side GDS seat retained
T19 41183.
T(iii) 41175

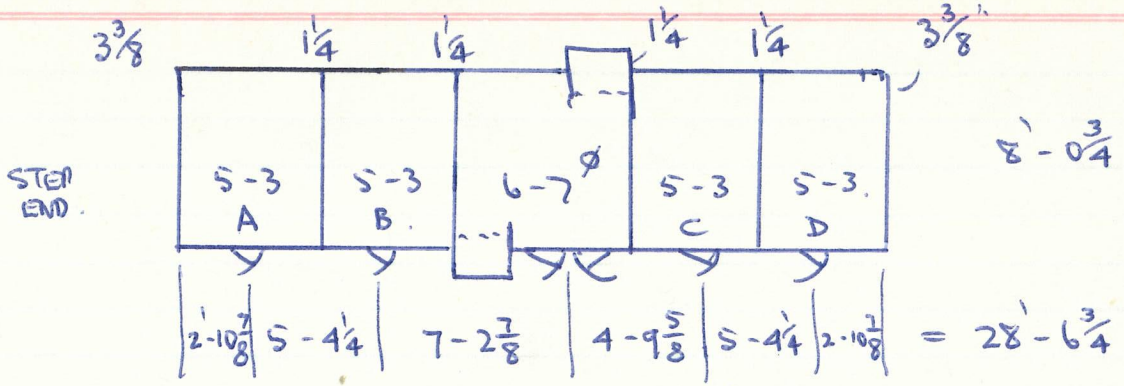
In each case:- ARC Roof 7'-3" high 4W. WB 19'-0 No horiz. end waist moulding.
Droplight in LH. D.D's RH. leaf has no droplight but does have weather bonnett in eaves panel.

The diff. bet. T19 & T(iii) is that in T19 the doors of the former L.H. couple of COMPTS. have been screwed up & three and four horizontal bars placed across them and in T(iii) the windows have all been panelled over.

T19 bars legend LOT 264 Nos 37 133 762 9 in m.s. PROJECTIONS removed on No 762 & 1 seat fitted See No 60524
T18 " " LOT 264 No 762 8 in m.s. " LOTS 162. 779. 133 748 LOT 264 No 142
ERROR PROB.
T(iii) " " LOT 264 Nos 142 & 748

For vehicles see T20.

T20
T21



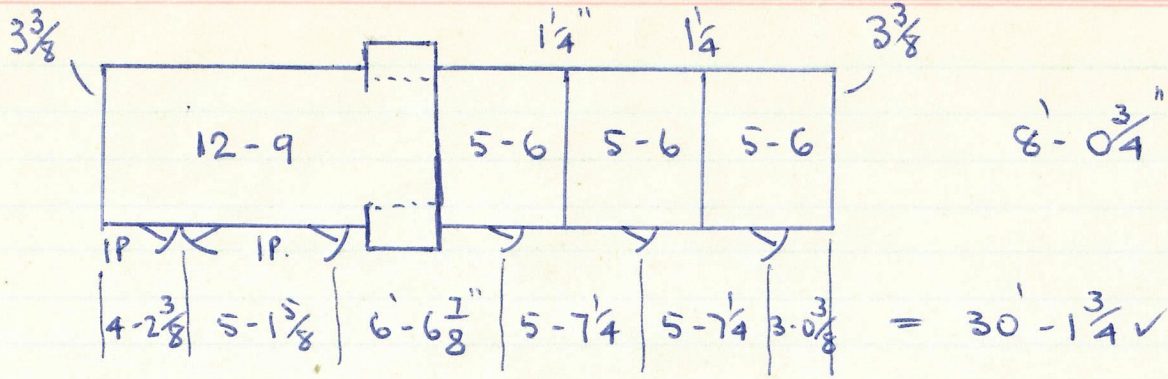
1/4" DIAG. T20 No-41165 T21 No-67333. WINGLESS FORM OF T20 New note GDS SEAT RETAINED
Note:- T18, T9, & T(iii) are 2 CPT conversions derived from T20
4W. NB 19' arc Roof 7'-3 high. No horizontal and waist moulding.
No droplight in R.H. leaf of DD's but there is a weather bonnet in the eaves panel
LOT 264 only. "N.G. Bke Bd. 10 entered "3994" 23-12-82

R.H. leaf is full 2'-0" size. on T38 it was only 1-11" wide.

T20 been no vehicle Nos but there is a note in m.s. "Registrations removed on No 162 - see Diagram No 67333.
T21 heard "Lot 264 No-162 & in m.s. "No-766"

Nos allotted 766, 142. 152. 762. 748. 37. 133. 779. 162. 907. (10).

T22
T 23.

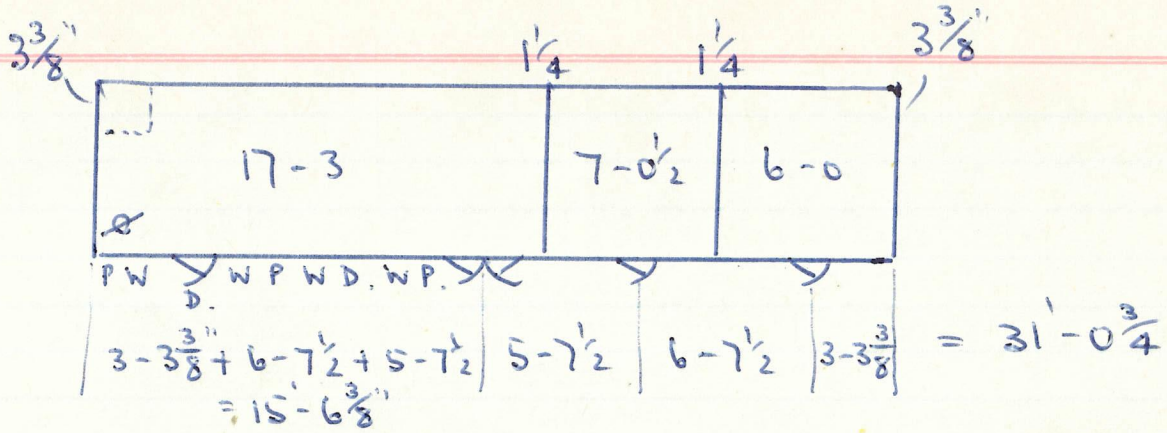


1/4" DIAG. T22 Nº 41176.
T23 Nº 73519 Winglen conversion of T22.
4W. WB 19'-0" 3 Centre Roof. 7'-6" high.
LOT 727 only.

NOTE:- 2 5/8" is the old measurement from a wing to a door opening & the principle here employed for siting the DD by allowing 2 5/8" from the GD's door opening and centering the DD. by halving what remains, is found in certain other designs.

T22 bears the legend in m.s. "Projections removed on Nº 2629.
T23. bears the legend "LOT 727 Nº 2629.

Only 2 vehicles Nºs 2629 & 2630 but this needs confirmation.



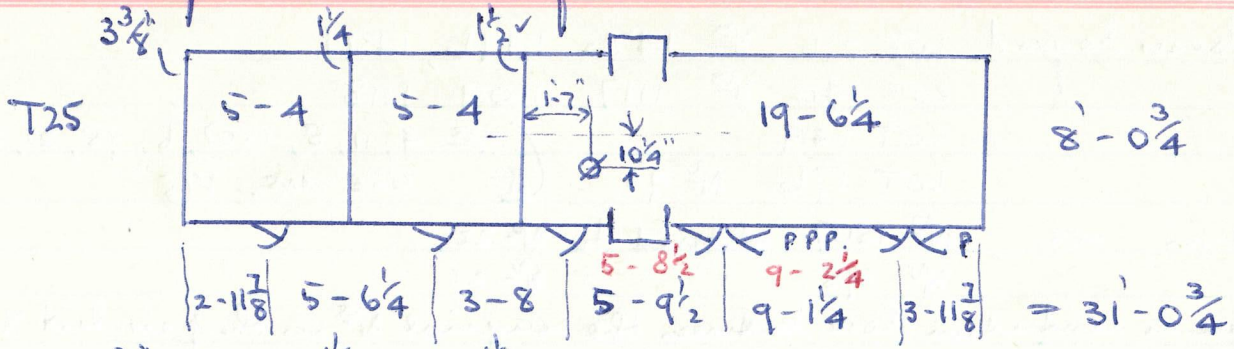
This is a conversion of 4.15. (Low window)
6 wheels WB 19.0. Arc roof 7'-3" high. No horizontal end waist mouldings.
No droplights in double luggage doors "3 and 4 horizontal bars" across the lights of the former compts. now thrown into the van ?over droplights of guard door also No!

1/4" Diagram No 33207. Lot 133 only.

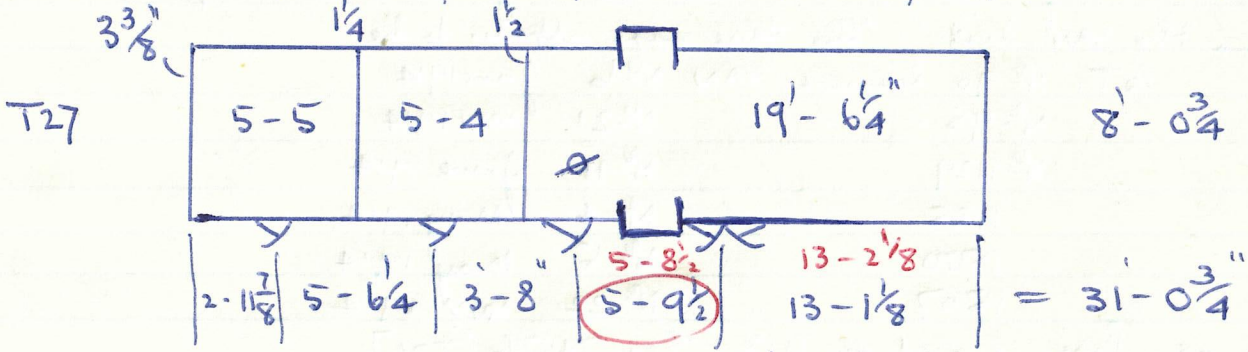
Diagram bears legend: "Lot 133. No 695"
and in MS: "No 695. Seats fixed in luggage cpt. for Workmen's use."

T25
T26
T27
T28

These 4 designs are conversions from T.38



T26 winglen T25 offside GD: ~~door~~ seat retained.



T28 is winglen T27 offside GD: seat. is retained.

1/4" DIAG.	T25	N° 36536.	6W	WB 19'-0"
	T26	74586	6W	- - - - -
	T27	41163.	4W.	- - - - -
	T28	60469	4W	- - - -

In each case ARC roof 7'-3" high – no horizontal end waist mouldings & presumably no droplights in the double luggage doors.

Note: As per later practice, the van partition has been increased to a thickness of 1 1/2"

Note that altho' drawn R.H.V. these 4 diagrams are, with relation to T38, actually L.H.V.

Loads of notes by Bunney, ignored! JR.

T25 brass legend" LOT 276 Nos 1476 1496, 1491
LOT 140 Nos 1117 1109 1118
LOT 149 (Nos 4, 11, 5, 1507, 6, 95, 21, 22. CONV. TO
LOT 176 No 93. & BKE VANS. "V/S"

& in m.s. No 1507 altered to No 22.
No 95 - - - - - No 21

Note:- There is confusion here between the original Nos in the pass. list & later Nos in the VAN list. The true case appears to be:-

BT. No 90 became	VAN	No 6	June 1914
No 95 -	- - -	No 21	NOV 1914
No 1129		No 11	June 1914
1135		No 4	May 1914
1500		No 5	May 1914
1507		No 22	NOV 1914

Also in m.s. Proj. removed on 1118 See 74586 [T26]

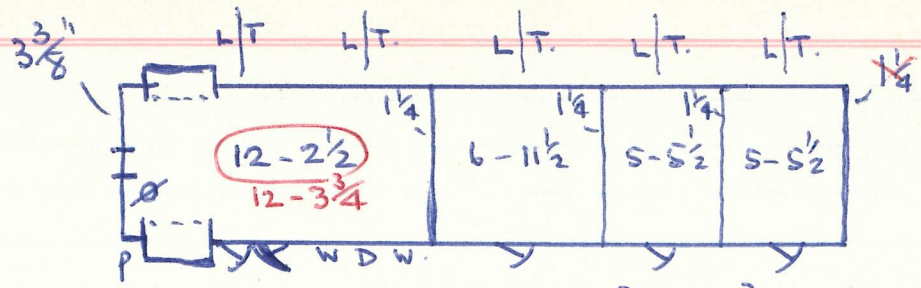
T26 brass in m.s. LOT 140 No 1111 (4W) 1124, 1101 (4W) 1134 (4W.
LOT 176 (178) No 96 (4W) 75, 65, 86, 93 (4W) 99 87 (4W) 69
LOT 149 No 1281 1274 1300 1293 1272 1279 1296 (4W) 1286 1282 (4W)

LOT 276 Nos 1105 1110 1137 1132 1109
and LOT 149 1301 1286, 1297 workmen.

T(ii) brass legend LOT 276 140 149 176 No VEHICLE Nos

T27 brass legend LOT 276 Nos 1475, 1482, 1490, 1494.
and in m.s. LOTS 140, 149, 176. Proj. removed No 1482

T28 { brass legend LOT 276 No 1482 LOT 149 No 1295
brass in m.s. LOT 149 Nos 1275 (bw) 1273 (bw) 1298 & 9 (bw)
1290 (4W) 1294 1285
LOT 276 Nos 1475 1496 1517 (bw) 1485 (bw) 1518 (bw) 1523 1509
LOT 176 Nos 104. 91. 87 (bw). 100 (bw) 125. 94. 68 69 (bw) 65 107
LOT 140 Nos 1120 1103 1118 1137 1110.



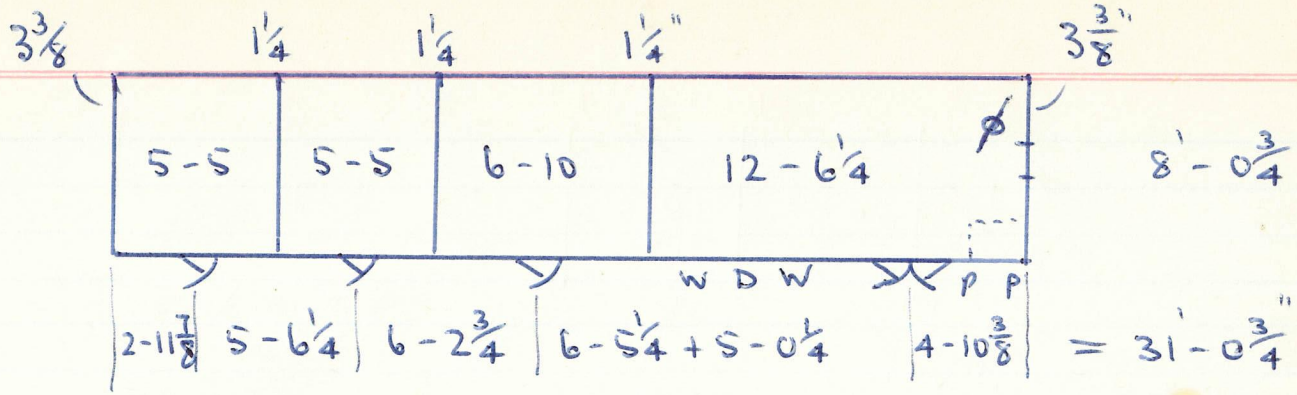
8' - 0 3/4"

4 - 1 1/2 5 - 5 7/8 + 6 - 6 3/4 6 - 3 3/4 5 - 6 3/4 3 - 0 1/8 = 31' - 0 3/4"

This is a declassing of a BKE TRI-COMPO built as a SLIP. of which T.40 is a variant still retaining the 2nd class CPT. 5'-11½" between PTN'S U17 in the 3-CPT COMPO DIAG. ¼ DIAG. Nº 26717
4W. WB 19'-0" originally 6 wheeled see U17.
ARC Roof 7'-3" high LOW WINDOW. NO HORIZ. END WAIST MOULDING.
LOT 138 Described as BKE/3RD. i.e. not SLIP.

T29 been in m.s. Nº 388 projection removed.
also appears on 4 CPT T.41
LVTG. ~~DOORS~~. 4'-0" on Nºs 372 373 see
IS A METRO
60915 (which is T.41), the wingless form of T.40

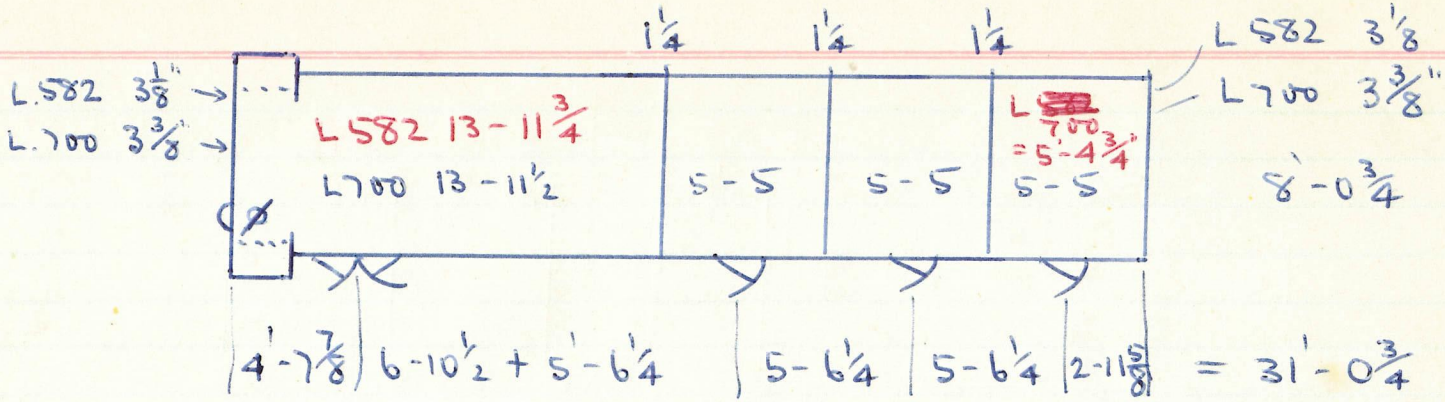
The R.H. leaf of the DD's on this DIAG. is about ½ the normal size & has neither deflight nor weather bonnet.
There are 4 HORIZ. bars across the light of the suppressed CPT., the door of which is screwed up. Nº 402 appears to be of this design.



This is a de-classing of 4.22 which in its turn is a conversion of a Tri-Compo Bke. built as a SLIP. T.30. is not described as a SLIP.
1/4" Diagram. No 60523.
6 wheels WB 22.0. Arc roof. 7-6 high High/low U22 is noted to have "shallow eaves panels"
Lot 349 only.

The wide panel to the right of the double doors covers the site of a former wing, there having been also a small panel between the wing and the end.
There was originally a Second Class Cpt which from the measurements on 4.22 may be calculated to have been 5.10 between ptns.
This has been thrown into the van, and the doors screwed up.

T.30 bears legend:- Lot 349 No 514 (511 added in ms.)
Also in ms. "No 4187, Lot 349 late Rd S.B. No 44".



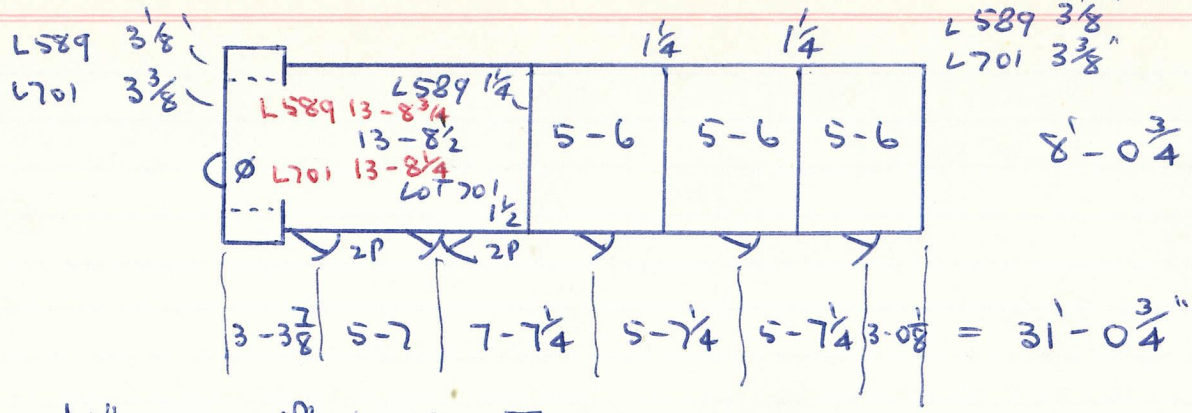
This is a conversion of a 4 CPT design T.49
¼" DIAG. № 41191
4 Wheels WB 19'-0" 3-Centre Roof. 7'-6" high
" 4 HORIZONTAL BARS across the windows of the cpt. now thrown into the room

LOTS 700 & 582 IN THAT ORDER.
NO vehicle numbers marked.

This DIAG. to be re-examined to see whether the wings commence near the base of the body or about half way up the bottom quarter panel LOT 582 is a border line case between the two practices.

Wings commence half-way up the body.

T32
T33

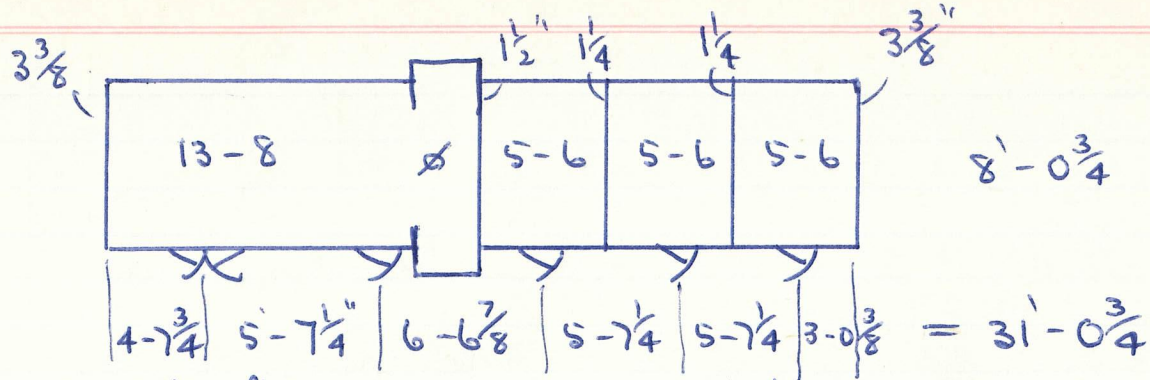


¼" DIAG. Nº 41182 T32.
Nº 60547 T33. winglen form of T32. off side 6di seat.
4W. WB 19'-0" 3-centre roof. 7'-6" high.
LOTS. 589 & 701 Described as N.G. BKE 3RD. (T32)

T33 has legend LOT 701 2606 & in ms. 2610 2612 2619 2618 441 and as part of the tracing LOT 589 Nº 918 and in ms. 912 913 946 330 919
Also in ms. Nº 946 LOT 589 Short buffers one end.

T38 to be re-examined to see whether the wings commence near the bottom of the body or about ½ way up the bottom ¼ panel.
It is a borderline case between the two practices.
(Longridge photo shows later type wing)
LOT 589 330 905 911 912, 913 918 919 935 946 997.
" 701 2606-2620 These were not the numbers originally allotted unknown what the originals were, or whether any vehicle bore them. Alteration made 10/9/94 i.e. about the time of the construction

T34
T35



1/4" DIAG. T34. Nº 41198 EX COMPO CPT 'C' 2ND class upholstery
T35 58878 winglers form of T(v) { 2 - PANELS IN LIEU OF WING.
T(v) 41192 ORIG. FORM ALL 3RD CLASS } NEAR SIDE GD'S SEAT.

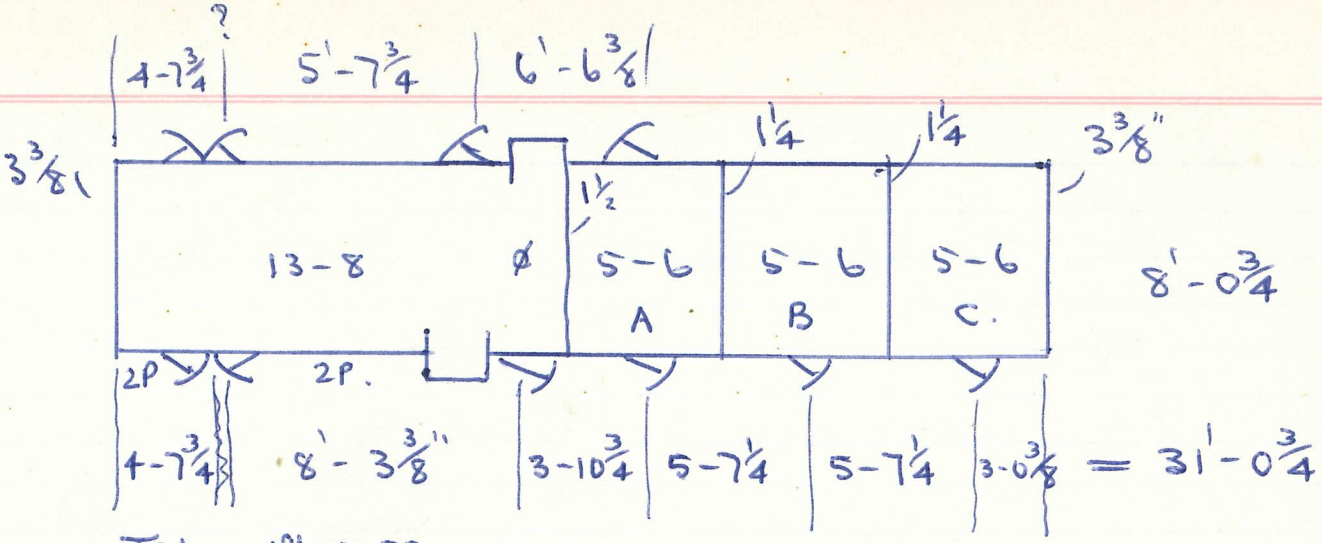
4W. W.B. 19' 3 Centre Roof 7'-6" high
LOTS 731 738 761 783.

T34. brass in ms. Nº 2691 fitted with droplight in VAN and APRIL 37.
Nºs 2677 2678 LONG BUFFERS. SCREW COUPLINGS ONE END.
Nºs 2654 2655 fitted chain roller bearings
The R.H. CPT. shows upholstery of Second Class character, & the LOTS on the DIAG are in this order. 761/731 738 783, Since lot 761 does not occur on T(v) it may be that only LOT 761 (or some of LOT 761 were even converted to compb.

T(v) bears the following lots 731. 783. 738. in that order.
T35 " legend & ms. notes:-
LOT 761 Nº 2690 + in ms. 2677, 2678.
LOT 738 Nº 2659 - - - - 2660. 2661. 2658. 2670. 2657.
in ms. LOT 731 2633 2634 2635 264x 2641 2632.
in ms. LOT 783 329 42.

LOT 731 2631 - 2650
738 2651 - 2670
761 2673 - 2692
783 Believed originally 3401 - 3404 Later apparently 42. 329. 914. 920
329 was ex - second 40 (ex 3rd 3403)

T36
T37



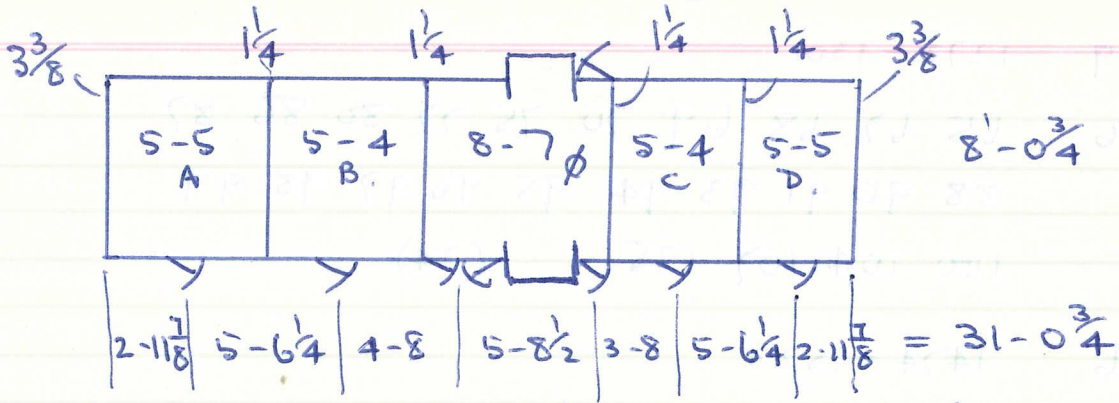
¼" DIAG. T36 № 41173.
T37 63799 Wingles version of T.36. Hdr seat on side
4W WB. 19'-0" 3 Centre Roof 7'-6" high.
LOT 978 only. Differs from T.34 in having staggered mullions.

T36 been in m.s. № 949 LOT 978 Proj removed & one seat fitted for guard.
№ 950 altered to № 45 R&S.B. Rly.

T37. been in m.s. № 942/№ 4188
LOT 978 Late R&S B. Rly. № 45

LOT 978 942. 949. 950. 951 952. 953. 954. 955. 956. 957

T38
T39



1/4 Dwg. No 41166 T38 The basic original design.
60470 T39. The swingler, altered design. No side GD's seat only

Note:- T25 - T28 are 2 cpt. designs altered from T.38.
6w. WB. 19'-0" ARC ROOF 7'-3" high No horizontal waist moulding
No deplights in luggage doors.
LOTS. 140 (part of) 149. 176. & 276.
PLENTY NOTES. BY BINNEY NOT COPIED.
T38 beam legend. N.G. BKE/3RD. LOTS 140. 149. 176. 276.
LOT 276 fitted with iron underframe.
No 130 LOT 140 centre wheels removed. + in ms. No 1295 LOT 149; centre wheels removed; also proj. removed + seat fitted for GD.
No 1499 Proj. removed. No 1504 short buffers.
LOT 276 No 1505 short buffers.
T39 beam legend. LOT 276 No 1499 & in ms. LOT 276 No 1510(4W) 1477 1484. 1481 1493 1483
LOT 176 No 97 98(4W)
LOT 140 1125 1130(4W) 1102 1108 1139 1128
~~1116(4W)~~ LOT 149 1292(4W) 1222(4W) 1288/87/76(4W) 1116(4W)
1278 1291
LOT 224. (ERROR)

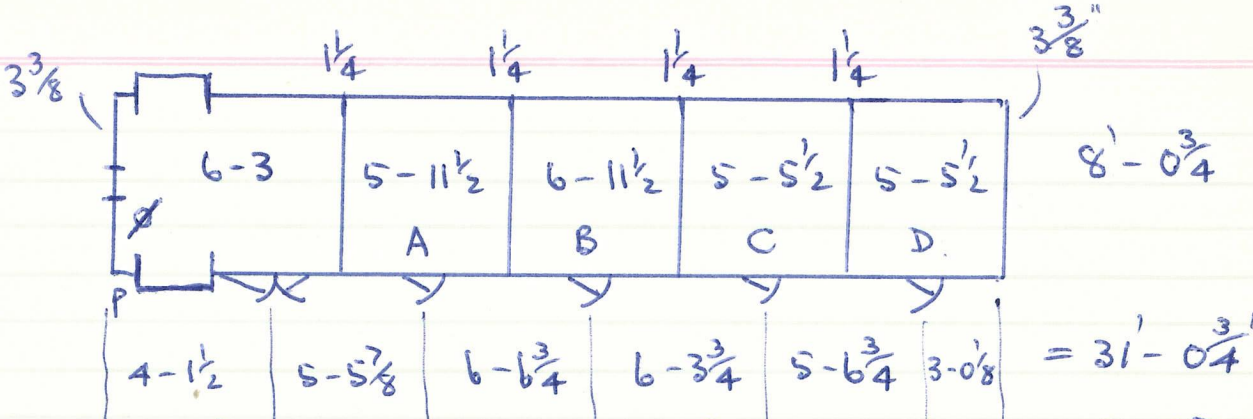
LOT 140 1100 - 1139 (19.53 also built on this LOT.)

149 1272 - 1301

176 65 67. 68 69 70 75 77 80 86 87
88 90 91 93 94 95 96 97 98 99.
100 104 107 125 (24)

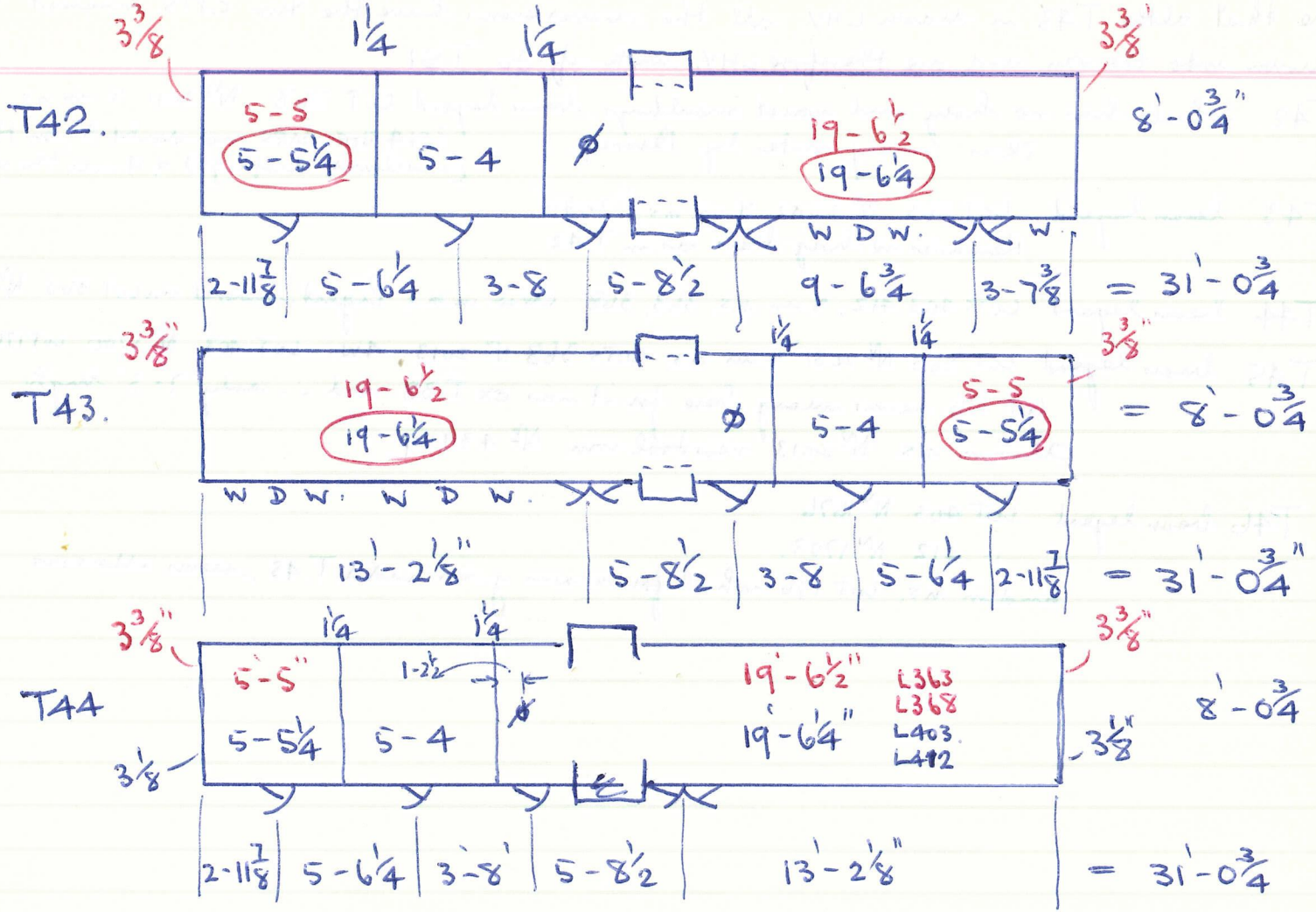
276 1474 - 1523.

T40
T41



This is a declassing of a TRI-COMPO BKE built as a slip (SEE U17 WINDOW IN END OF GDS CPT)
1/4" DIAG. T40 No 26716.
T41 No 60915 Winglen version of T40. off side seat for G.D.
See also T29 for conversion with CPT. 'A' thrown into the van.
4W. WB 19'-0" possibly originally six wheeled.
ARC ROOF 7'-3" high. LOW WINDOW. NO HORIZ. END WAIST MOULDING.
LOT 138 only. Described as 'SLIP' Third class.

T.40 been in m.s. Nos 373 (372?) & 378 METRO
T41 been legend LOT. 138 Nos 372, 378, ✓ in m.s. Nos 402, 407.



ALL 5 Designs are conversions from T.51

1/4" DIAG.	T42	№ 41187	6W.	WB. 22'	2/DD.	ARC ROOF HIGH/LOW.	7'-6" HIGH
	T43	41189	6W	22'	1/DD.	- - - - - - - - - -	
	T44	41168	6W.	WB { L403. 22' / L412 20'	1/DD.	3 Centre Roof High.	- - - - -
WINGLESS T44	T45	63792	6W.	WB. 22'-0	1/DD.	- - - - - - - - - -	
	T46	41180	4W.	WB { L403. 22' / L412 22'	1/DD	- - - - - - - - - -	

note that altho' T43 is drawn LHV. all the conversions have the two CPTS. nearest to the DD: thrown into the van and are therefore LHV. with ref to TS1

T42. which has no horiz. end waist mouldings, beam legend LOT 368 № 2011 & in m.s. 2042. plus many notes by Bunney. 3 & 4 HOR. BARS. no doubt 3, on the (shallower) dustlight & 4 over the deeper ¼ light

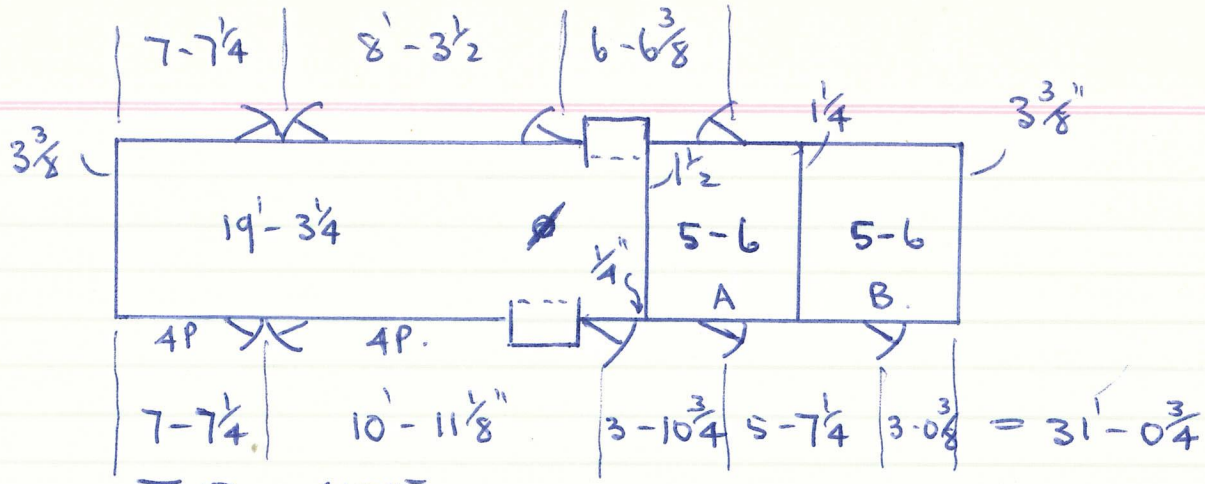
T43. beam legend LOT 363. № 2042 & in m.s. "2030" Remarks re horiz. bars as in T.42.

T44 beam legend LOTS 403 412, & in m.s. 363, 368 also in m.s. Project. removed on LOT 363. № 2031

T45 beam legend LOT 363 № 2031 & in m.s. LOTS 368 № 2013 4W. LOT 403 № 1700 LOT 176 № 107 WORKMENS LOT 176 seems wrong here for it was ex T.38 and ∴ only 7'-3" high. Also in m.s. № 2013 new tool van № 43909

T46 beam legend LOT 403 № 1676
" 412 № 1703.
and in m.s. LOT 176 which for reasons given under T.45, seems erroneous.

T47
T48



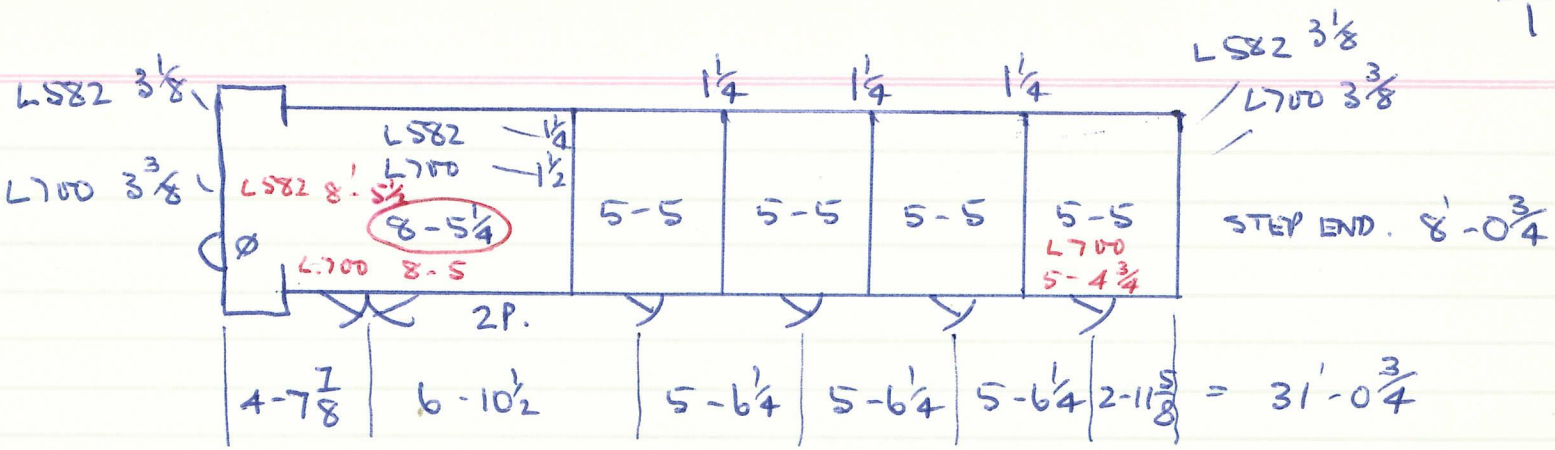
¼ DIAG. T47 41185
T48 72938 Wingless version of T47. Offside G.D.'s seat.
4W. WB. 19'-0 3-centre roof 7'-6 high.
LOTS. 946 991 1013. (The latter two added in m.s.)

T47. is marked in m.s. "Projects removed on Nos 945 958 See DIAG. 72938

T48 has legend LOT. 946 No-945 & in m.s. 948
LOT 991 No-958 & in m.s. 971
& in m.s. LOT 1013. No-353.

LOT 946 940, 941 943 944 945 947 948 (7)
Ruabon & Dolgelly Branch Trains
991 958 959 960 961 962 963 964 965 966 967.
968 969 970 971 972. (15)
1013 314 353 357 358 360 (5)

T49
T50



1/4" DIAG. T.49 No. 39042
T50 60522. Wingless version of T.49. off side GDs seat.
See also T.31. No. 41198 Conversion to 3 CPTS.
4W. WB. 19'-0" 3-Centre Roof 7'-6" high. LOTS 582 & 700

T49 is described as N.G. Brake 3rd. in ms. it is marked:- Projects removed and one seat fitted for Guard on LOT 582 Nos 429 462 See also 60522

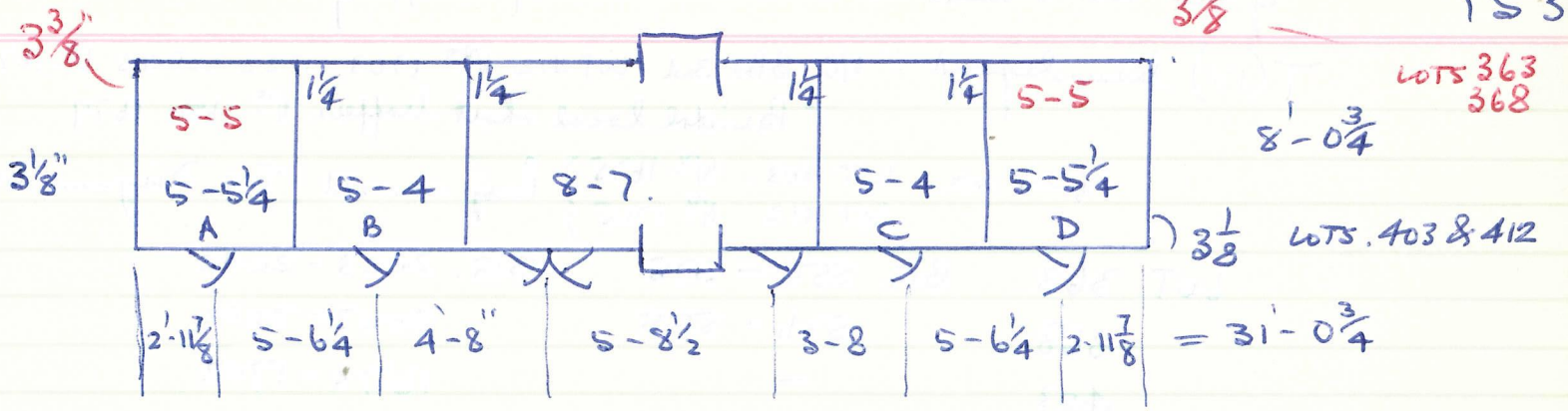
T50 bears legend LOT 582 Nos 429 462 & then in ms. 414 420, 411, 419 442 444 475 410 476 LOT 700 No. 2601

T49 to be re-examined to see whether the wings commence near the bottom of the body or about 1/2 way up the bottom 1/4 panel. It is a borderline case between the two practices
Wings commence half way up the body.

LOT 582 400 410 411 413 414 416 419 420 422 427
429 442 444 453 461 462 463 464 475 476 (20)

LOT TWO 2601 - 2605

T51
T52
T53.



¼ DIAG. T51 Nº-? ~~[illegible]~~ LOTS 363 & 368 (in m.s. 403 & 412) ARC ROOF HIGH/LOW NO HOR. END WAIST MOULDINGS. VI

T52 Nº-41256 LOT 412 CPTS' C & D. UPHOLS. AS 2ND CLASS 3 Cent Roof High WINDOW. HOR. END MOULD.

T53. 60548 WINGLESS VERS. OF T.51 3 C. Roof High Window. HORIZ. END WAIST MOULDINGS LOTS AS T51

T (iv) 43586 AS T.52 but WB. 22' 6W. & CPTS B & C 5'-5"

6W. W.B. VARIOUS. Roof 7'-6 against 7'-3" of T38 & CONVERSIONS.

There appears to have been a consolidation of DIAG.'s here as the following will suggest

LOT 363. Ends should be 3 3/8 ARC Roof HIGH/LOW WB 22'

368 -

403. - - - - - - - 3⅛ 3 Cent. Roof High W. WB 22'

412 - WB 20' only.

T51 { been legend LOTS 363 & 368 and in M.S. 403 412.
in m.s. Proj. removed on LOT 368 Nº 2015 2021 See Drg 60548

T52 been legend LOT 412 Nº 705 (or similar Nº illegible

T53 been legend LOT 403. Nº 1675 1678 Centre wheels removed on 1678

& in m.s. LOT 403. 1677 1687 1692 1702.

LOT 412 1715 802 1721 1708 1712 1705 1722 1720.

LOT 368 2004 2015 2021 2017 2014 2020.

LOT 363 2035

The ARC High/Low form is identifiable on a B.G. Corridor Type II see photo of old B.G. vehicles at SWINDON awaiting scrapping.

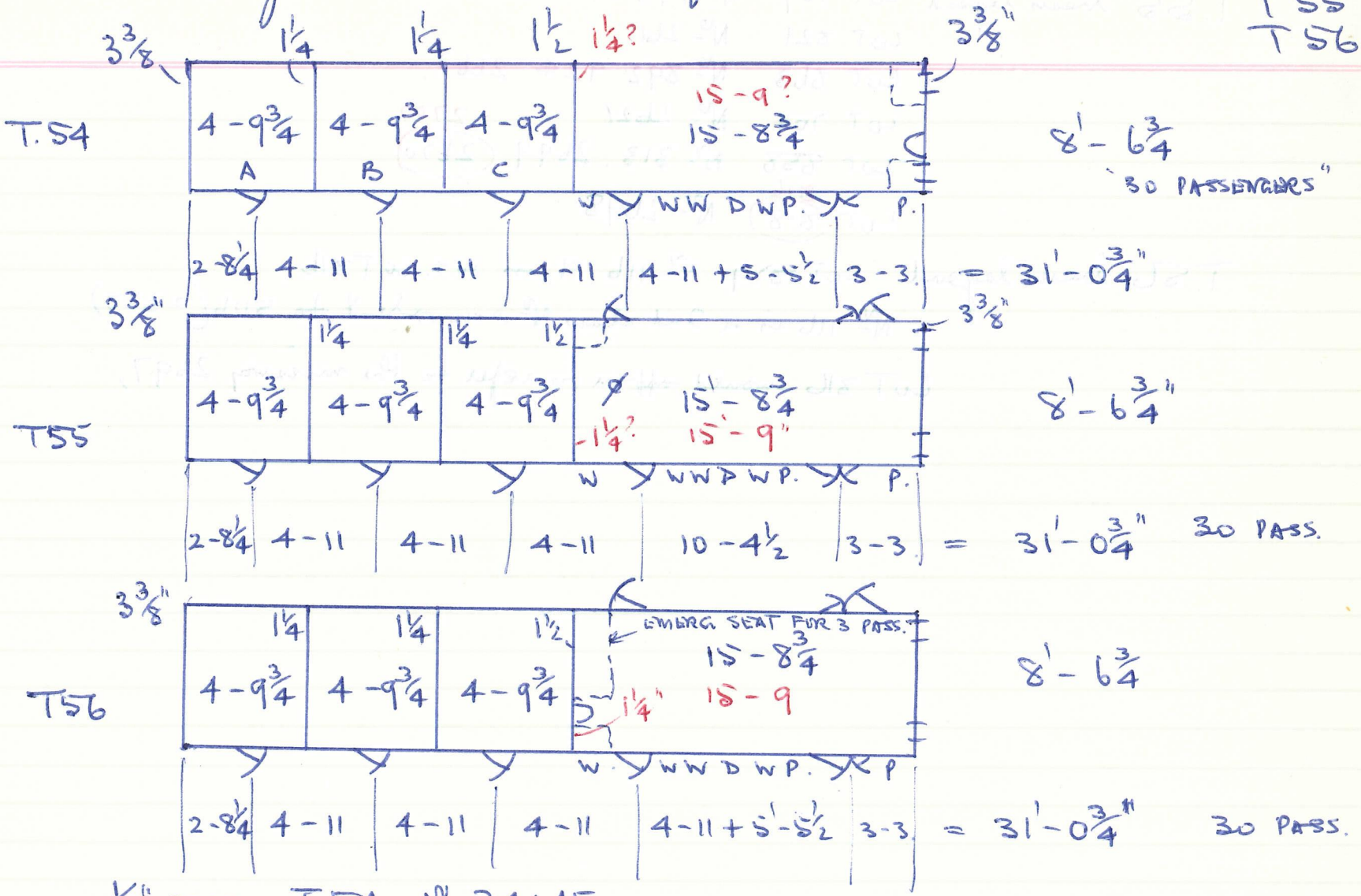
T(iv) beam legend " N.G. Bbe/3rd LOT 412 Nos 1703 - 1722 LOT 403 No - 1673 - 1702
" Bristol local short buffers. Nos 1680 1679.

& in m.s. LOT 403. No - 1675 } Proj. removed See Diagram 60548(T53)
LOT 412 No - 1715 }

LOT 363.	B.G. 556 - 575	S.G. 2023 - 2042
368	536 - 555	2003 - 2002
403	—	1673 - 1702
412	—	1703 - 1722.

These 3 diagrams represent conversion of T.59.

T54
T55
T56



1/4" DIAG. T.54 № 34145
55 33199
56 32497.

4W WB. 19' - 3 Centre Roof. 7' - 3" high.
windows in van end wall.

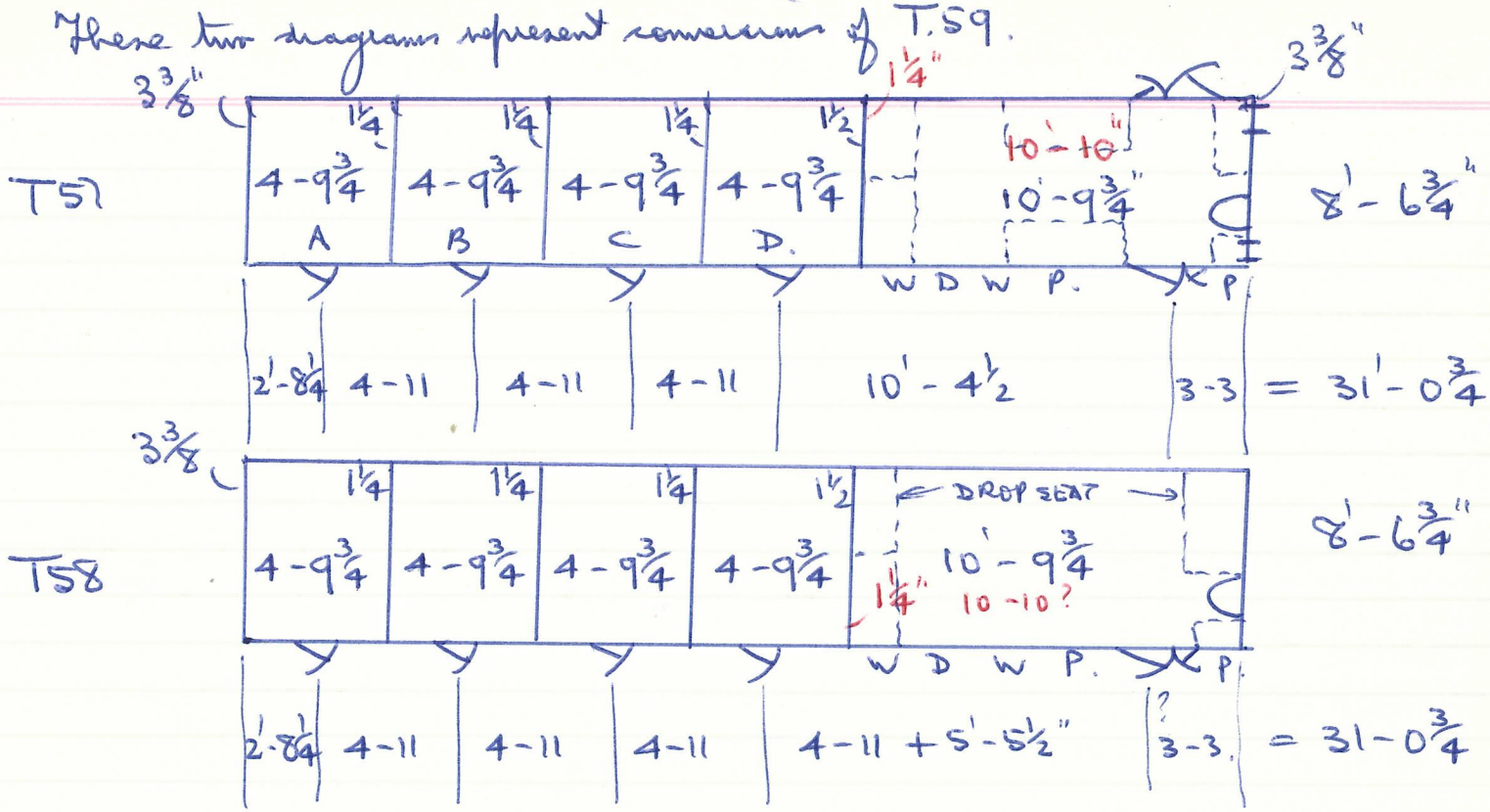
T54 been legged. LOT 709 704. 874 879 № 253. LOT 879 short coupled one end

T.55 bears legend LOT 709 № 794 & in m.s. "1671"
LOT 821 № 2611
LOT 863. № 892 924 2667.
LOT 704 № 2621
LOT ~~858~~ 816 № 313. 2699 2670 2700
LOT ~~818~~ № 2698

T.56 bears legend LOT 819 № 116 & in m.s. LOT 816.
№ 116 is a 2nd class № equivalent to 5116 (= 338)
LOT 816 would appear to refer to the missing 2697,

T57
T58

These two diagrams represent conversions of T.59.



1/4" DIAG. T57 41162 T58 No- 32485

4W. WB. 19'-0" 3-centre roof. 7'-3" high. Windows in van ends walls.

T57 beam legend. LOTS 711 777. 805 816 858 874 897 909 & in m.s.

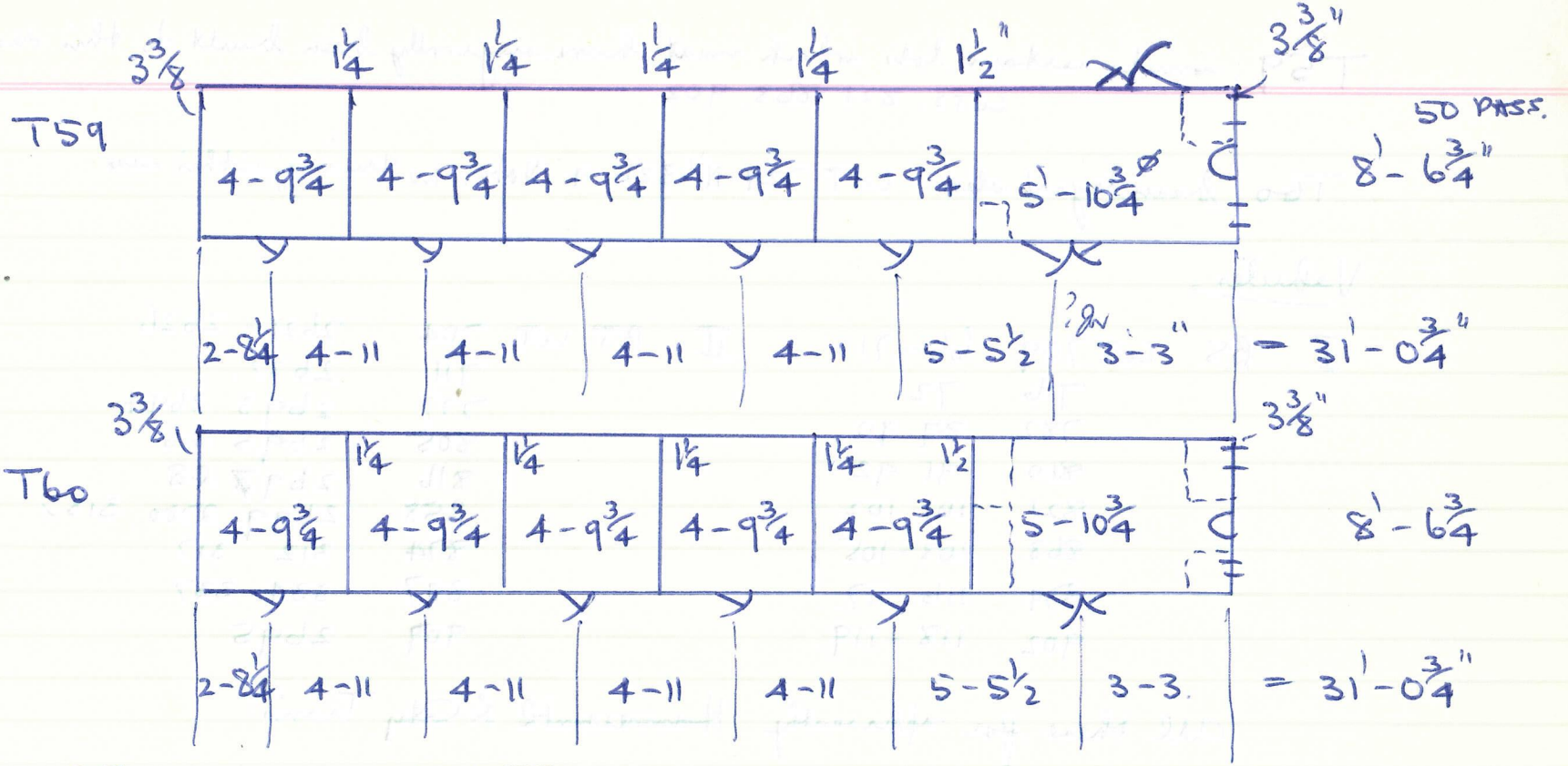
LOT 874 No- 312 See T58 No- 2695 LOT 902 52 PASS.

Note that altho' No- 2695 appears in the register as NOV 1898 which is correct for LOT 902, there appears to be too many BKE vehicles

T58 beam legend LOT 821 No- 115 EX B.S. 5101

LOT 879 No- 253. EX B.S. 5117.

LOT 902 No- 628, 694. [EX B.S. 5118/19]



T60 an alteration of T.59.
1/4" DIAG. T59. No- 41169
LOTS. 704 709 710 782 810 879 816 858 874 897 771 711 & 805
T60 No- 41170
LOTS. 709 716 782 810 879.
4W. WB. 19'-0" 3- Centre Roof 7'-3 high. Windows in VAN ENDS.
Quarter lights appear to be slightly narrower than the std 13 3/4 but they have individual holection mouldings. The RH. leaf of the DD's about 3/4 full size has neither droplight nor ventilator

T59. omits certain lots which must have originally been built to this design
LOTS. 821 863. 902.

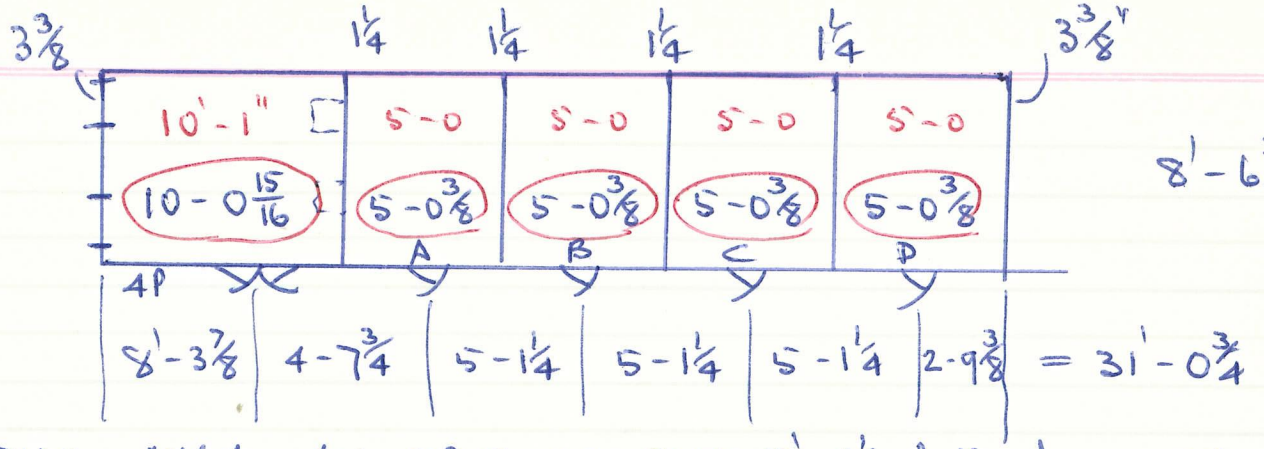
T60 has legend also LOT 729 No-326 & Note. No trimmings in this case

Vehicles

I	B.S.	LOT 709	66-71	II	BT	LOT 704	2621-2626
		716	72			711	2627.
		782	89 90			777	2693 2694
		810	91 92			805	2695 &6
		821	101 102			816	2697 &8
		863	103-105			858	2699 2700 313 ✓
		879	116-117.			874	312 317.
		902	118-119.			897	334 337.
						909	2695

all these for apparently Hammersmith & City Trains.

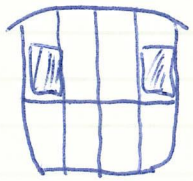
T61



8'-6 3/4

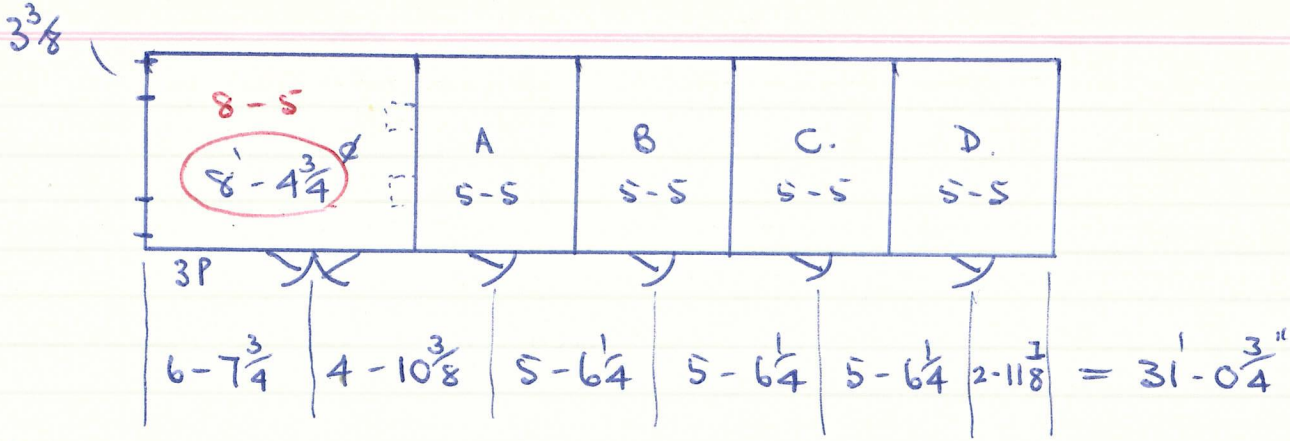
1/4" DIAG. 41164 4W WB 19 ARC ROFF 7'-4 1/2 high. from rail level 11'-4 1/2
LOT. 317 This is the BKE/3RD. of order No-1 & shares the same LOT No with the 5??
The DIAG. beam legend LOT 307 No-507 also in m.s. LOT 149 (This appears nonsense for LOT.149 is B.T. with centre van only 8'-0 3/4" wide & which 149 is a 5/3RD.

END ELEVATION.



HORIZ. END MOULDING IS SUSPECT LOT 354 didn't have it LOT 356 is the earliest known for certain to have it.
This order belongs to the low window period but has high windows extending almost to the EAVES.

T61 beam legend LOT 317 No-507 & in m.s. LOT 149 (which seems erroneous)

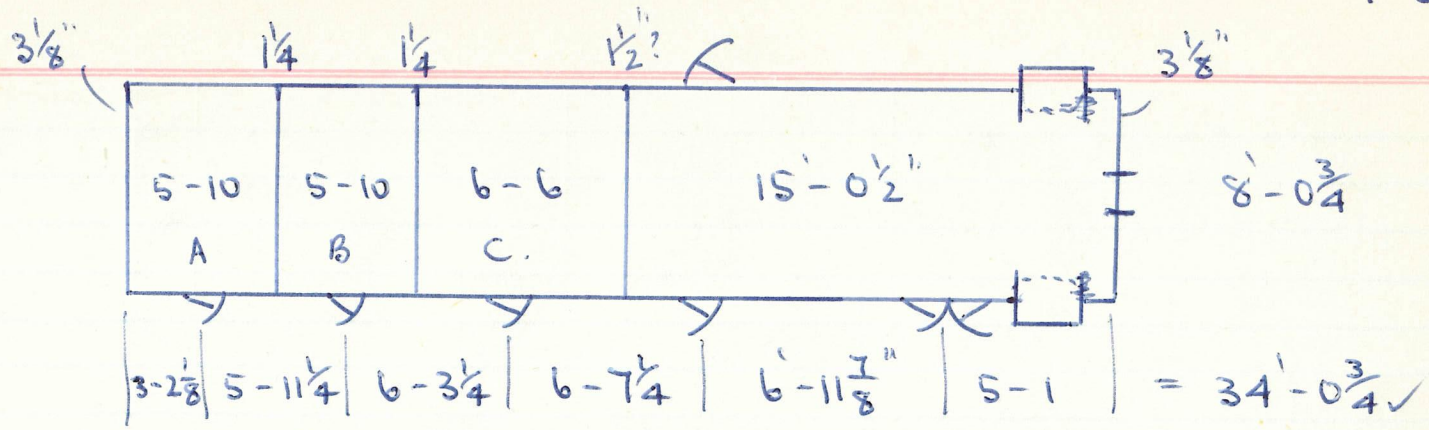


1/4" DIAG. No not ascertainable.
4W. WB. 19' ARC Roof 7'-4½" high from rail level 11'-4½
LOT 319

Drag. beam legend. LOT 319 No. 479.
ex 2nd 169 4/99.

For end elevation see T61.

T63



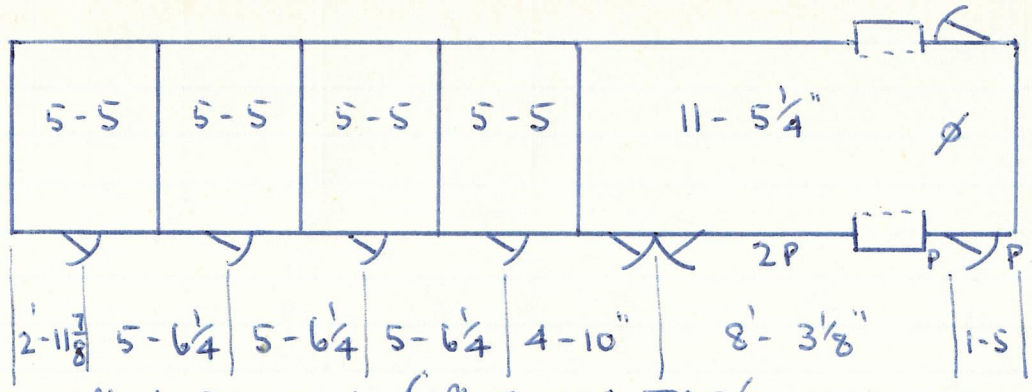
This is a declassing and a conversion of a BKE 2ND/1ST COMPO U(V) built on a SLIP. 1/4" DIAGRAM No. 41373A. [No. 41373 is U(V)]
6 Wheels WB. 22'-0" 3 - Centre Roof 7'-6" high. High Window
LOT 410 only.

Several notes by Binney referring to discrepancy of 1/4" (See 1 1/2" PTN. ABOVE)

T63 bears legend "LOT 410" and in ms. No. 519.
Also in M.S. "LOTS 138, 390" which are however, of a different length & design.

ENDS	6 1/4
3 PTNS x 1 1/4	3 3/4
2 CPTS x 5'-10"	11-8
1 " 6-6	6-6
VAN	15-0 1/2
	34-0 1/2
Defic.	1/4"

If the figures are correct, then the PTN between CPT C and the van is 1 1/2", but it should be noted that LOT 410 is a great white before any 1 1/2" PTN, is established to appear in any new design (LOT 601) & that the removed PTN between CPT 'D' and the van should have been only 1 1/4"



¼" DIAG. № 41171 T64 (№ 60916 T65 (WINGLESS VERSION OF T64) NEAR SIDE GD'S SEAT
6 WHEELS W.B. 22'-0" 3 CENTRE ROOF 7'-6" HIGH. "HIGH/LOW" LOT 390 ONLY

Check on internals :-

ENDS	6¼"
4 PTNS @ 1¼	5"
4 CPTS x 5'-5"	21 - 8
VAN	11 - 5¼
	34 - 0½

DEFICIENCY ¼"

It would appear that the VAN PTN was 1½" thk. tho' long before this increased thickness has hitherto been established.

This is suspicious :-

c/L CPT D to c/L DD.		4 - 10
less ½ CPT x 5-5	2 - 8½	
LH. leaf of DD.	2 - 0	
PTN say	1½	
		4 - 10
		NIL

" Clearance therefore
Yet a clearance of at least ¼" is usual.

Note T65 has m.s. marks indicating that both the doors at the extreme R.H. end, are hinged on the side nearest the ~~do~~ END.

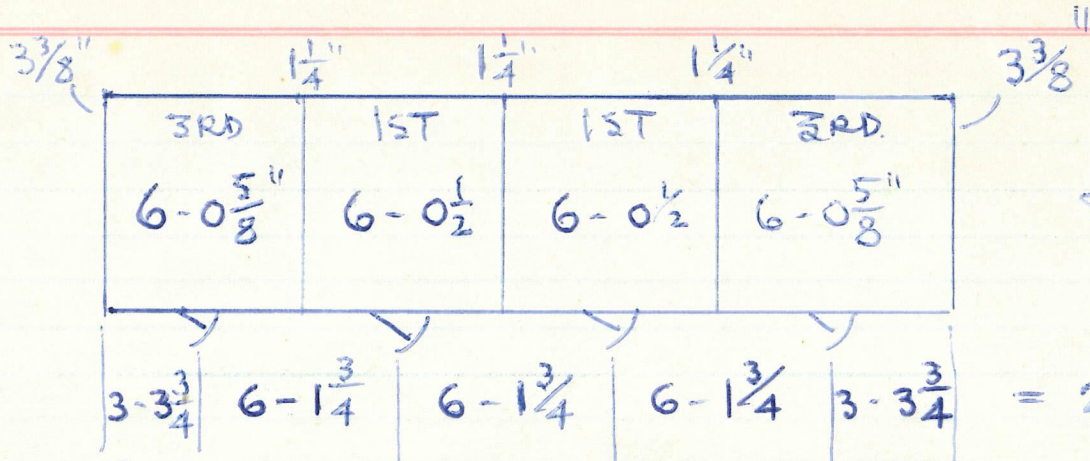
T64 bears in m.s. "Projection & one seat removed on № 1984, 1982? SEE 60916

T65 bears legend "LOT 390 № 1982 and in m.s., "1977, 1979 (4 wheels).

Vehicles

B.G. 510 - 515 S.G. 1977 - 1982.

N.B. 513 not traced in list of conversions to S.G.



METRO

8'-6 3/4"

= 25-0 3/4"

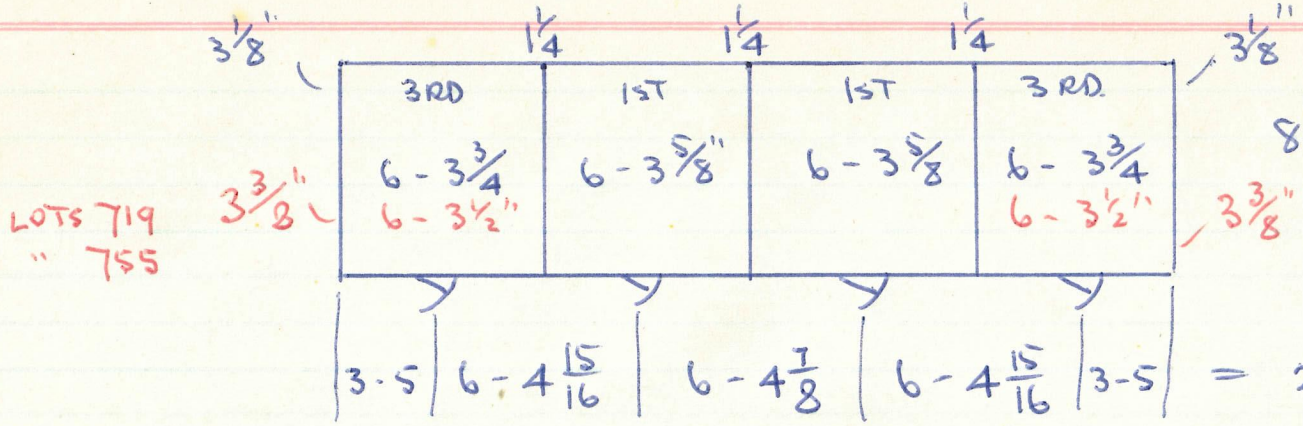
This design is a declassing of FIRST. R3.

1/4" DIAG. Nº 338XX [ILLEGIBLE]

4 WHEELS, WB 16'-0" 3-CENTRE ROOF 7'-3"/11'-2 1/4"

NO VEHICLE Nºs MARKED.

U2
U(i)



8' - 6 3/4" METRO

This design is a declassing of the FIRST. R5 (and R.4)

1/4" DIAG. Nº 27484 (U2) 3 11 3 4W. WB 17'-0 3 - Centre Roof — /11-2"
Nº 33213 U(i) 1 11 3.

("BODY HEIGHT PRES. 7'-3" - NOT MARKED EITHER ON THIS DIAGRAM OR ON R5 or on U(i).

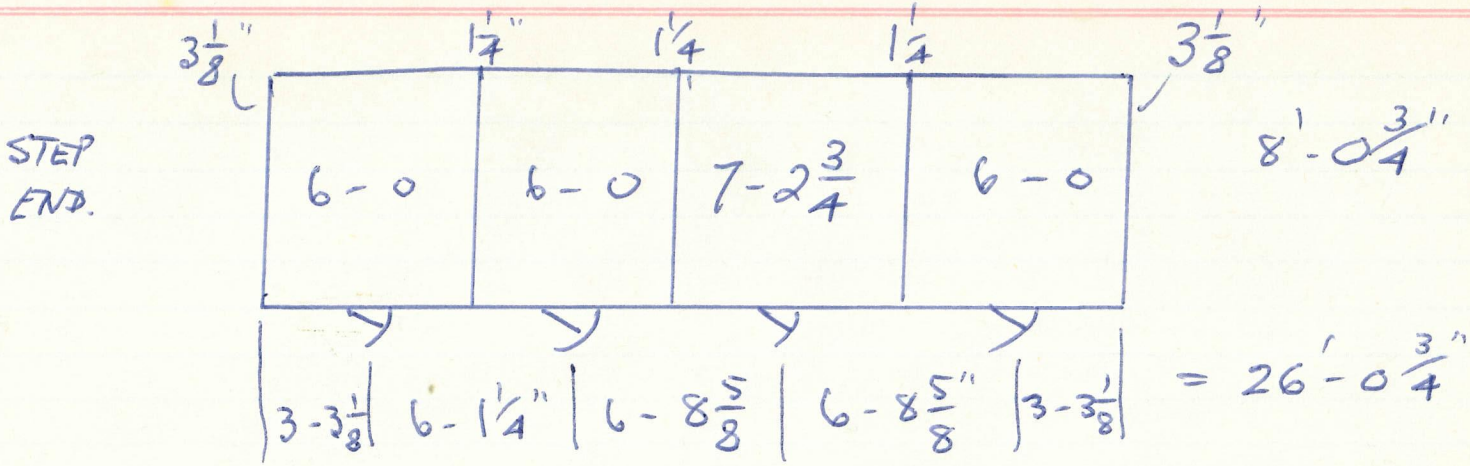
Check of intervals as for R5
Note that Lots 719 & 755 should have 3 3/8" ENDS. and the end CPTS. therefore. 6' - 3 1/2" ONLY.

LOTS 479 394 426 501 719 755 Note that LOT 394 is ex-R.4.

U(i) differs in having only one Third-Class. CPT, the R.H. end CPT. 2X bears legend, "Combo LOT 755 Nº 6291
" ORIGINAL FIRST CLASS - See 1st Class Roll for Tracing"

18 1st Cl. Seats.
8 2nd Cl. "

U3

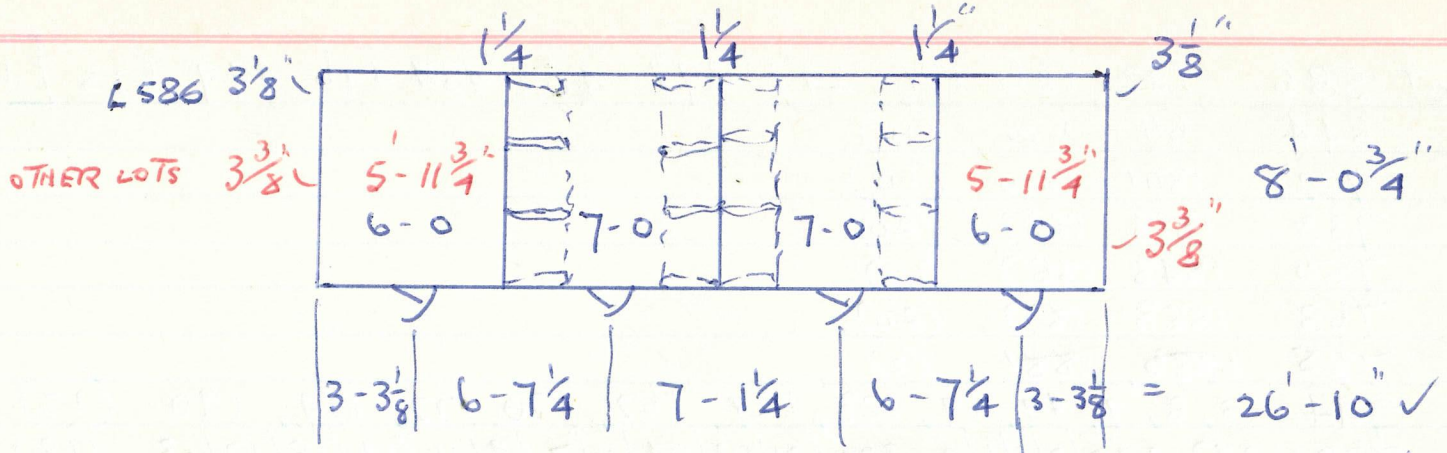


1/4" DIAG. No. 40733

4 W. WB. 17' - 0" 3 - Centre Roof. 7' - 6 / 11 - 5 1/4"

LOT 506 ONLY. BUILT AS. CONVERTIBLE TYPE II B.C. Nos 592, 593
N.G. 6599, 6600
"FOR LAUNCESTON BRANCH"

Note:- This is a 'local' and put into service a considerable time before any (others?) 2x. Therefore it was not part of any order for complete trains. 2x was preceded by certain 5/3RDS with 3-centre roof, but these appear to have been indented for 'through trains', as they ran on 6 wheels



¼" DIAG. Nº 31923. 4W. WB. 17'-0" 3 Centre Roof. 7'-6/11-5¼" "NG. COMPO

LOTS 586 644 702 733 750 763 944 990 1012 785
"why 785 out of order?

Only LOT 586 should have thin ends. All the other lots should have 3⅜" ends & the end CPTS. should be 5'-11¾". The DIAG. is however, marked in ms. 8'-0⅛" which appears incorrect for any LOT.

ORIGINALLY 2.1.1.2 afterwards 3.1.1.3. But the diagram bears a deleted '2' against both 1st class CPTS. NO vehicle appears in the 2nd class list with this length but perhaps a 3.2.2.3. vehicle at some time.? The DIAG. bears in ms. :- LOT 644 Nº 6614 "SHORT BUFFERS"
702 7807 " "
733 7834 " "
586 6297 " "

E Hawkesworth has offered photo of this vehicle which has solebars painted - - - [?] Lyobowen to BALA.

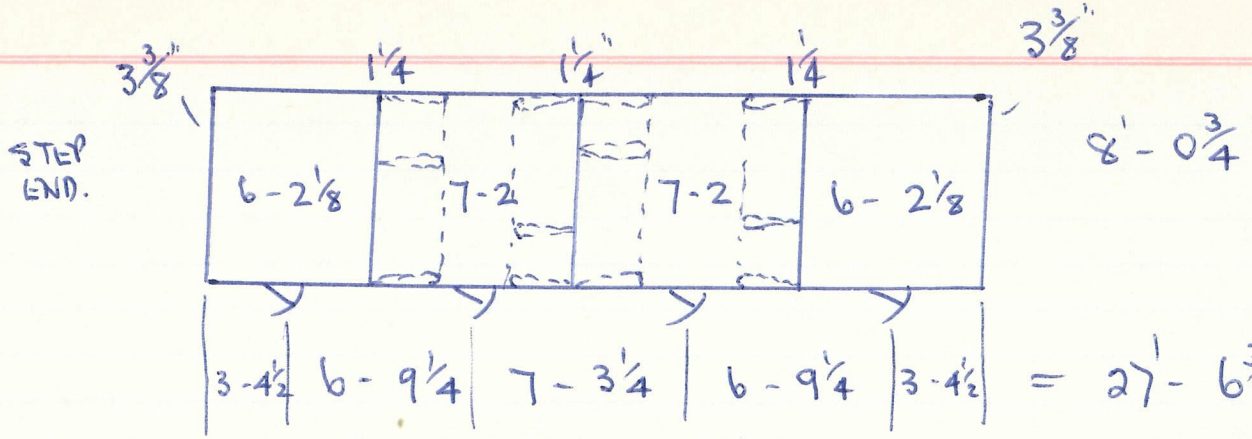
P.T.O. ↓

LOT 586 132 136 171 297 397 482 484 496 499 623 [10]
" 644 611 – 620 (10)
702 1801 – 1810 (10)
733 1818 1837 (20)
750 1848 1857 (10)
763 1858 – 1877 (20)
785 1880, 1881 (2)
944 3, 8, 9, 49, 59, 128, 129, 162, 170, 172, 174, 190 (12)
990 247, 290/2/3/6/9, 300/01/02/04/6/7/8/11/14/15/17/18 (18)

1012 230*

Some confusion here. apparently LOT 990 was to have been of 15, & LOT 1012 of 5. A vehicle seems to be missing.

U5



1/4" DWG Nº 40732.
4 WHEELS. W.B. 18'-0" 3 - Centre Roof. 7'-6/11-5 1/4"

LOT 728 ONLY

DIAG bears in m.s. " Nº 6621, chain roller bearings fitted MAY 1924
removed after trial.
SKEFCO ROLLER BEARINGS FITTED OCT 1928 Cones:
Nº 21809 MALMESBURY BRANCH

Nºs 621, 622.